



# CONTRACT DRAWINGS FOR TOWN OF SCHROEPPLEL AINSLIEE DRIVE SEWER DISTRICT OSWEGO COUNTY, NY AUGUST 2021 FINANCED BY USDA RURAL DEVELOPMENT AND NYSEFC #C7-6341-02-00



NEW YORK LOCATION MAP  
SCALE: NTS



PROJECT MAP

SCALE: NTS

### TOWN OFFICIALS

SUPERVISOR	LYNETT GRECO
TOWN CLERK	DARLENE OWENS
COUNCILPERSON	WILLIAM EDWARDS
COUNCILPERSON	TIMOTHY DUNNIGAN
COUNCILPERSON	LEWIS HOYT
COUNCILPERSON	NICKLAUS HOYT
ATTORNEY	KEVIN CARACCIOLI
HIGHWAY SUPERINTENDENT	CLIFFORD HOYT

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REVISIONS		
REV	DESCRIPTION	DATE
FOR RECORDED		AUGUST 2021

PROJ. #: 170003

DATE: AUGUST 2021

SHEET

G-001



RECORD DRAWINGS  
THESE DRAWINGS WERE PREPARED IN ACCORDANCE WITH CONSTRUCTION RECORDS AS PROVIDED BY THE CONTRACTOR. THEY SHOW THE PROJECT "AS-CONSTRUCTED" TO THE BEST OF OUR KNOWLEDGE AND BELIEF BUT ARE NOT GUARANTEED TO BE CORRECT IN EACH AND EVERY DETAIL.  
C2AE





WE RECYCLE

Table with 2 columns: REVISIONS, REV DESCRIPTION DATE. Includes project number 170003 and date AUGUST 2021.

TYPICAL WORK ZONE TRAFFIC CONTROL NOTES

- 1. WORK ZONE TRAFFIC CONTROL SHALL BE PROVIDED IN ACCORDANCE WITH THE NATIONAL MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS - LATEST EDITION AND THE NEW YORK STATE SUPPLEMENT.
2. PRIOR TO THE START OF WORK, THE CONTRACTOR MUST SUBMIT ANY PROPOSED CHANGES TO THE TRAFFIC CONTROL PLAN TO THE ENGINEER FOR APPROVAL.
3. THE TRAFFIC MAINTENANCE SCHEMES SHOWN IN THE NATIONAL MUTCD AND THE NEW YORK STATE SUPPLEMENT DESCRIBE THE MINIMUM METHODS AND CONTROL DEVICES NECESSARY. THE ENGINEER MAY ORDER ADDITIONAL DEVICES AND/OR METHODS TO MEET FIELD CONDITIONS. NO ADDITIONAL PAYMENT WILL BE MADE FOR ADDITIONAL DEVICES SO ORDERED.
4. DIAMOND-SHAPED SIGNS SHALL BE USED FOR ALL ADVANCE WARNING SIGNS SHOWN IN PART 6 OF THE NATIONAL MUTCD ALONG WITH THE NEW YORK STATE SUPPLEMENT. COLOR REQUIREMENTS SHALL BE BLACK TEXT ON ORANGE BACKGROUND. COLOR REQUIREMENTS FOR GUIDE SIGNS AND REGULATORY SIGNS USED FOR CONSTRUCTION SIGNING SHALL BE AS SPECIFIED IN THE NATIONAL MUTCD AND THE NEW YORK STATE SUPPLEMENT FOR EACH SIGN, OR AS NOTED ON THE PLANS.
5. THE CORRECT SEQUENCE AND SPACING OF SIGNS, EITHER PERMANENT OR TEMPORARY MUST BE MAINTAINED AT ALL TIMES IN ACCORDANCE WITH THE NATIONAL MUTCD AND THE NEW YORK STATE SUPPLEMENT, UNLESS SHOWN OTHERWISE ON THE PLANS.
6. ALL SIGNS, INCLUDING GUIDE SIGNS, SHALL INDICATE ACTUAL CONDITIONS AT ALL ITEMS AND SHALL BE COVERED, MOVED, REMOVED OR CHANGED IMMEDIATELY AS DIRECTED BY THE ENGINEER.
7. WORK ZONE TRAFFIC CONTROL SHALL BE PROVIDED IN ACCORDANCE WITH THE MOST RECENT NYSOT STANDARD SPECIFICATIONS-SECTION 619-3.02K WORK ZONE TRAFFIC CONTROL. THE NATIONAL MUTCD AND THE NEW YORK STATE SUPPLEMENT AND ANY PROVISIONS CONTAINED IN THE PLANS AND/OR PROPOSAL OF THIS PERMIT OR AS ORDERED BY THE AUTHORITY HAVING JURISDICTION.
8. WORK ZONES IN OPPOSITE SIDES OF THE ROAD SHALL NOT OVERLAP. WORK ZONE IS DEFINED AS THAT AREA IN WHICH TRAFFIC IS RESTRICTED BECAUSE OF CONSTRUCTION ACTIVITIES, OR THAT AREA WHICH INVOLVES A DROP-OFF NEXT TO THE PAVEMENT.
9. CONSTRUCTION EQUIPMENT SHALL BE REMOVED FROM THE CLEAR ROADSIDE AREA DURING NON-WORKING HOURS. PROVIDE A MINIMUM 30 FOOT CLEAR OFFSET.
10. NO MATERIAL IS TO BE PLACED ON THE SHOULDER OR WITHIN THE CLEAR ROADSIDE AREA (DETERMINED AS IN NOTE 9) EXCEPT THAT WHICH IS TO BE PLACED THAT DAY.
11. PRIVATE VEHICLES OWNED BY THE CONTRACTOR OR HIS WORKMEN SHALL NOT BE PARKED ON THE PAVEMENT OR THE SHOULDERS, OR ANY OTHER LOCATION CONSIDERED BY THE ENGINEER TO BE A HAZARD.
12. THE CONTRACTOR SHALL PROVIDE ONE-INCH STEEL PLATES TO PROVIDE FOR TRAFFIC MOVEMENT OVER NARROW, OPEN EXCAVATIONS. EXCAVATIONS MADE FOR THE INSTALLATION OF THE PIPES WILL BE BACKFILLED AT THE CLOSE OF EACH DAY. TEMPORARY PAVEMENT SHALL BE PLACED NO LATER THAN END OF WORK ON THE FOLLOWING DAY.
13. NO DROP-OFF GREATER THAN SIX INCHES SHALL BE LEFT OVERNIGHT WITHIN 30-FOET OF THE EDGE OF PAVEMENT. DROP-OFFS LESS THAN SIX INCHES SHALL BE PERMITTED IF PROPER DELINEATION AND SIGNING IS PROVIDED.
14. W20-7a "FLAGGER" SIGNS SHALL BE USED WHENEVER FLAGGING OCCURS FOR MORE THAN A BRIEF PERIOD OF TIME. THE SIGNS SHALL BE PROMPTLY REMOVED, COVERED, OR FACED AWAY FROM TRAFFIC WHEN THE FLAGGING OPERATIONS CEASE.
15. THE FLAGGING STATIONS AND LAND CLOSURES SHOULD BE LOCATED TO ENSURE MAXIMUM VISIBILITY.
16. ALL FLAGGERS SHALL USE STOP/SLOW PADDLES (NO ROUND PADDLES) WHERE EVER PADDLES CAN BE USED. FLAGS SHOULD ONLY BE USED WHERE PADDLES MAY BECOME CONFUSING. AT INTERSECTION CONTROL, STOP/SLOW PADDLES USED AS SIGNALING DEVICES SHALL COMPLY WITH SECTION 6E.03 HAND-SIGNALING DEVICES OF THE NATIONAL MUTCD AND THE NYS STANDARD SPECIFICATIONS SECTION 729-05. THE STOP/SLOW PADDLE SHALL BE A MINIMUM OF 24" (600mm) WIDE AND SHALL HAVE 8" (200mm) HIGH LETTERS.
17. SIDEWALKS SHALL BE REPLACED PRIOR TO MOVING TO THE OPPOSITE SITE TO WORK.
18. ALL SIGNS THAT ARE REMOVED DURING CONSTRUCTION OPERATION SHALL BE REINSTALLED AS SOON AS POSSIBLE (WITHIN THE SAME DAY). ANY DAMAGE TO SIGN PANEL OR POST SHALL BE REPAIRED AT NO COST TO THE AUTHORITY HAVING JURISDICTION. ANY SIGNS THAT NEED TO BE TEMPORARILY RELOCATED AS A RESULT OF THIS PROJECT MUST BE REPLACED IN THE CORRECT LOCATION, ELEVATION, AND AT THE CORRECT ANGLE TO TRAFFIC BY THE END OF THE DAY. THE BREAKAWAY SYSTEM ON ANY REPLACEMENT SIGNS SHALL MATCH THAT OF EXISTING SIGNS. IF AT ANY TIME A STOP SIGN MUST BE REMOVED, A FLAGGER SHALL BE USED TO CONTROL TRAFFIC AT THE INTERSECTION UNTIL THE STOP SIGN GETS REPLACED.
19. CARE SHALL BE TAKEN TO INSURE THAT NO DAMAGE OCCURS TO THE EXISTING PAVEMENT/SHOULDER/CURB AREAS AS A RESULT OF CONSTRUCTION EQUIPMENT MOVEMENT.

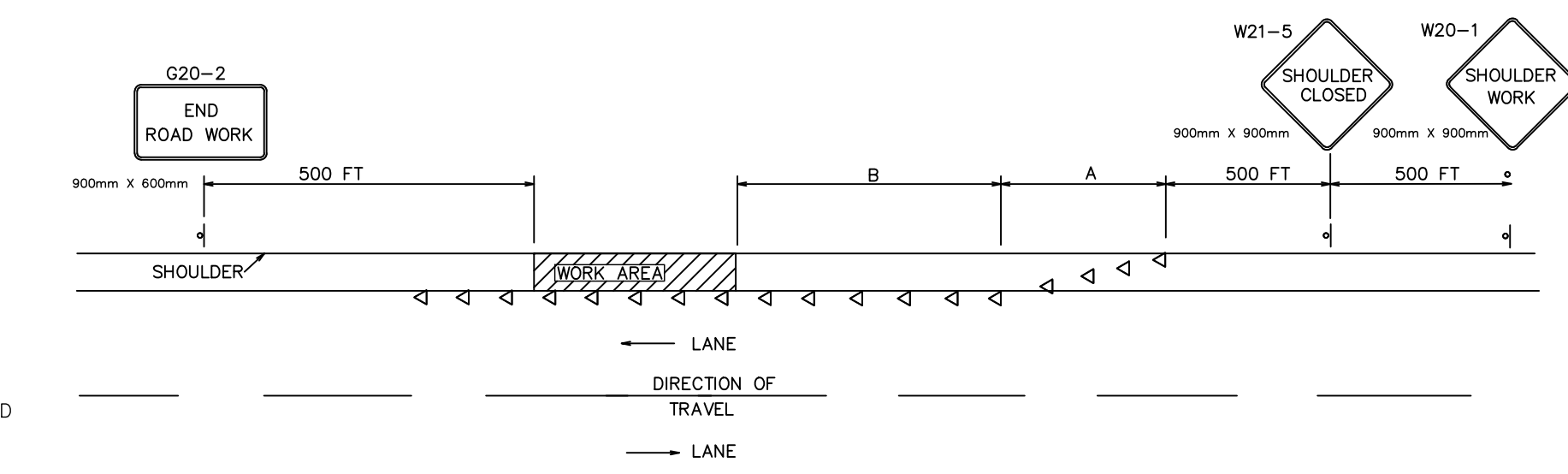


Table with 4 columns: SPEED (MPH), SHOULDER WIDTH, A, B. Includes legend for cones, work area, and sign.

NOTES:
"A" = TRANSITION LENGTH, SEE TABLE
"B" = BUFFER ZONE, SEE TABLE
THE MAXIMUM SPACING BETWEEN CHANNELIZING DEVICES SHALL BE 40'.
ALL SIGNS SHALL HAVE ORANGE BACKING WITH BLACK LETTERING AND TRIM.
ANTICIPATED WORK DURATION, 1 HOUR TO 12 HOURS, DAYTIME N.Y.
THIS DETAIL IS FOR SHOULDER WORK ONLY, NOT FOR WORK IN TRAVEL LANE(S).

TRAFFIC CONTROL FOR SHOULDER CLOSURE ON ALL HIGHWAYS

- NOTES FOR SHORT OR INTERMEDIATE TERM STATIONARY LANE CLOSURE FOR 2-LANE 2-WAY ROADWAY
1. WHEN PAVED SHOULDERS HAVING A WIDTH OF 8' OR MORE ARE CLOSED, CHANNELIZING DEVICES SHALL BE USED TO CLOSE THE SHOULDER IN ADVANCE TO THE BEGINNING OF THE WORK AREA.
2. WHEN A SIDE ROAD OR DRIVEWAY INTERSECTS THE ROADWAY WITHIN A WORK ZONE TRAFFIC CONTROL AREA, ADDITIONAL TEMPORARY TRAFFIC CONTROL AREA, ADDITIONAL TEMPORARY TRAFFIC CONTROL DEVICES AND/OR FLAGGERS SHALL BE PLACED AS NEEDED. ADDITIONAL FLAGGERS SHALL BE LOCATED AT ALL INTERSECTIONS AND COMMERCIAL DRIVEWAYS LOCATED WITHIN OR NEAR THE ACTIVE WORK SPACE.
3. NO WORK ACTIVITY, EQUIPMENT, OR STORAGE OF VEHICLE, OR MATERIAL SHALL OCCUR WITHIN THE BUFFER SPACE AT ANY TIME.
4. CHANNELIZING DEVICE SPACING (CENTER TO CENTER) SHALL NOT EXCEED 40' IN THE ACTIVE WORK SPACE.
5. TRANSVERSE DEVICES SHALL BE REQUIRED (AS PER NYSOT 619 STANDARD SPECIFICATIONS) WHEN A PAVED SHOULDER HAVING A WIDTH OF 8' OR GREATER IS CLOSED FOR A DISTANCE GREATER THAN 1500'.
6. THE END ROAD WORK SIGN (G20-2) SHALL BE PLACED A MAXIMUM OF 500' PAST THE END OF THE WORK SPACE.
7. WHERE DIRECTED BY THE ENGINEER, A BUFFER SPACE SHALL BE PROVIDED IN ORDER TO LOCATE THE ONE-LANE, TWO-WAY TRAFFIC TAPER PRIOR TO ANY HORIZONTAL OR VERTICAL CURVE, IN ORDER TO PROVIDE ADEQUATE SIGHT DISTANCE FOR THE FLAGGERS AND/OR A QUEUE OF STOPPED VEHICLES.
8. THE FLAG TREE SHALL BE LOCATED ON THE SHOULDER, AT APPROX. 1/2 THE DISTANCE BETWEEN THE FLAGGER SIGN (W20-7a) AND THE FLAGGER.
9. FLAGGER SIGN (W20-7a) AND ONE LANE ROAD AHEAD SIGN (W20-4) SHALL BE REMOVED, COVERED OR TURNED AWAY FROM ROAD USERS WHEN FLAGGING OPERATIONS ARE NOT OCCURRING.
10. FLAGGER AND FLAG TREE SHALL BE ILLUMINATED TO LEVEL 2 ILLUMINATION DURING NIGHT TIME OPERATIONS.
11. ALL FLAGGERS SHALL USE 24" (MIN) OCTAGON SHAPED STOP/SLOW PADDLES HAVING 6" STAFF.
12. CENTERLINE CHANNELIZING DEVICES ARE OPTIONAL AND MAY BE ELIMINATED WHERE SPACE CONSTRAINTS EXIST.

WORK ZONE TRAFFIC CONTROL LEGEND

Table with 2 columns: SYMBOL, DESCRIPTION. Includes direction of traffic, arrow panel, channelizing device, sign, work space, and work vehicle.

URBAN: (MEETS MORE THAN 1 OF THE FOLLOWING CRITERIA) SIDEWALKS, BICYCLE USAGE, CURBING, CLOSED DRAINAGE SYSTEMS, DRIVEWAY DENSITIES GREATER THAN 24 PERCENT, DRIVEWAYS/AL OR GREATER, MAJOR COMMERCIAL DRIVEWAYS, NUMEROUS RIGHT OF WAY CONSTRAINTS, HIGH DENSITY OF SIDEWALKS, 65+ PERCENT SPEEDS OF 45 MPH OR LESS.
RURAL: ANY AREA NOT EXHIBITING MORE THAN ONE OF ABOVE CHARACTERISTICS.
EXPRESSWAY: DIVIDED HIGHWAYS FOR THROUGH TRAFFIC WITH FULL OR PARTIAL CONTROL OF ACCESS AND GENERALLY WITH GRADE SEPARATIONS AT MAJOR CROSSROADS.
FREEWAYS/INTERSTATE: LOCAL OR INTERREGIONAL HIGH-SPEED, DIVIDED, HIGH-VOLUME FACILITIES WITH FULL OR PARTIAL CONTROL OF ACCESS.

GENERAL NOTES

- 1. NO SIGNS SHALL BE REMOVED FROM THE RIGHT-OF-WAY.
2. ROAD TO BE KEPT CLEAN OF MUD AND DEBRIS AT ALL TIMES
3. ROADSIDE DRAINAGE TO BE MAINTAINED AT ALL TIMES
4. MATERIALS, EQUIPMENT AND VEHICLES ARE NOT TO BE STORED OR PARKED WITHIN THE RIGHT-OF-WAY.
5. NOTIFY DIG SAFELY NEW YORK THREE WORKING DAYS PRIOR TO DIGGING, DRILLING, OR BLASTING AT 1-800-962-7962, FOR A UTILITY STAKE-OUT.
6. ALL WORK CONTEMPLATED AND MATERIALS USED WITHIN THE RIGHT-OF-WAY SHALL BE COVERED BY AND IN CONFORMITY WITH THE LATEST EDITION OF THE NYS DEPARTMENT OF TRANSPORTATION SPECIFICATIONS BOOK AND ANY SUBSEQUENT ADDENDA ALONG WITH ANY APPROPRIATE CURRENT NYS DEPARTMENT OF TRANSPORTATION STANDARD SHEETS, EXCEPT AS MODIFIED IN THESE PLANS AND IN THE ITEMIZED PROPOSAL. METRIC UNITS MAY BE CONVERTED TO ENGLISH.
7. QUALITY CONTROL OF ASPHALT CONCRETE SHALL MEET THE REQUIREMENTS OF SECTION 402 OF THE NYSOT STANDARD SPECIFICATIONS. ASPHALT COURSE DEPTHS SHOWN ON THE PLANS ARE COMPACTED DEPTHS.
8. NO NIGHT WORK WILL BE ALLOWED UNLESS PRIOR APPROVAL IS GIVEN BY THE AUTHORITY HAVING JURISDICTION. ADDITIONAL MAINTENANCE AND PROTECTION OF TRAFFIC WILL BE REQUIRED INCLUDING THE ADDITION OF REFLECTIVE MATERIALS AND LIGHTING.
9. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ANY HIGHWAY WORK PERMIT, ASSOCIATED BOND, AND SPECIAL INSPECTION FOR WORK WITHIN THE RIGHT-OF-WAY. THE TOWN SHALL BE CO-PERMITTEE FOR THE HIGHWAY WORK PERMITS.
10. ESTABLISHING TURF SHALL BE ACCOMPLISHED PRIOR TO OCTOBER 15TH. SHOULD THERE BE ANY OPEN AREAS AFTER THAT DATE, THOSE AREAS SHALL BE APPROPRIATELY PROTECTED FOR EROSION THROUGH THE WINTER PER SWPPP REQUIREMENTS.
11. THE CONTRACTOR SHALL BE RESPONSIBLE TO REPAIR ANY PROJECT RELATED SETTLING OF LAWN AREAS, SIDEWALKS, DRIVEWAYS, ROADWAY, AND ANY OTHER AREAS OF DISTURBANCE UNTIL FINAL CONTRACT ACCEPTANCE.

RECORD DRAWINGS
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C2AE
70 MAIN STREET, PO BOX 368, CANTON, NY 13617

HOLIDAY RESTRICTIONS

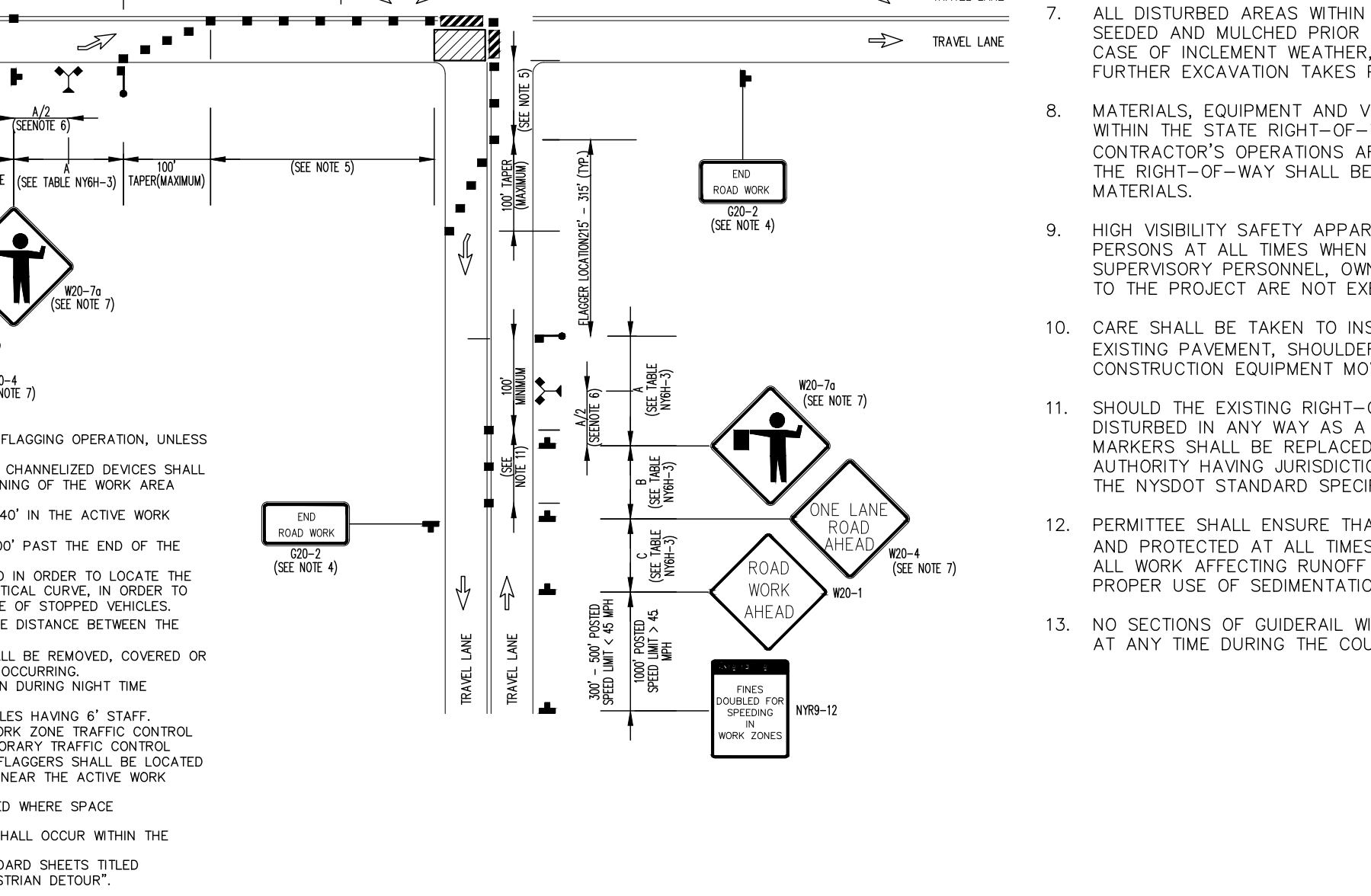
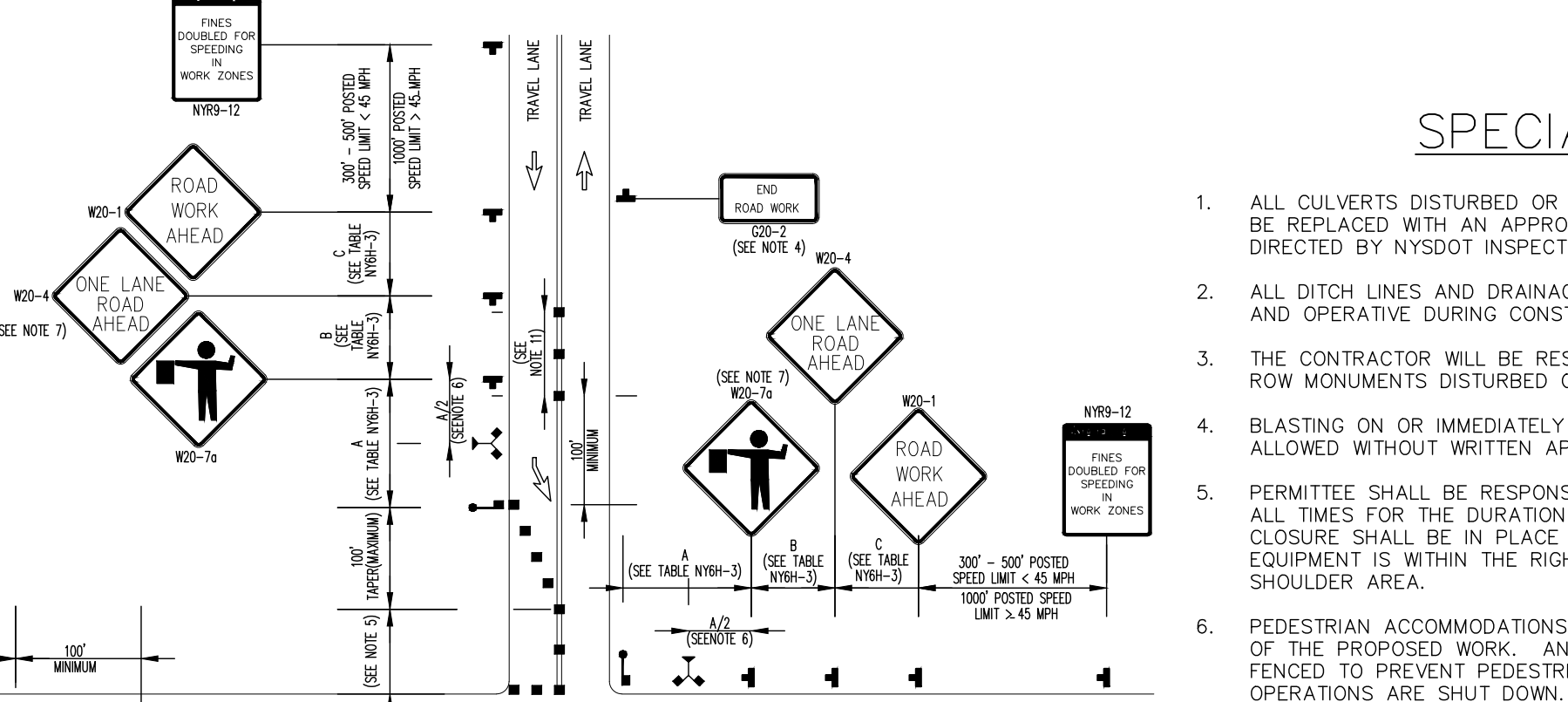
- SHOULDER CLOSURES AND LANE RESTRICTIONS ARE NOT ALLOWED DURING THE FOLLOWING DATES:
MEMORIAL DAY HOLIDAY - FRIDAY, MAY 23, 2020 THROUGH TUESDAY, MAY 26, 2020
INDEPENDENCE DAY HOLIDAY - THURSDAY, JULY 2, 2020 THROUGH MONDAY, JULY 6, 2020
OSWEGO HARBORFEST - THURSDAY, JULY 23, 2020 THROUGH SUNDAY, JULY 26, 2020
LABOR DAY HOLIDAY - FRIDAY, SEPTEMBER 4, 2020 THROUGH MONDAY, SEPTEMBER 7, 2020
THANKSGIVING HOLIDAY - WEDNESDAY, NOVEMBER 25, 2020 THROUGH MONDAY, NOVEMBER 30, 2020
CHRISTMAS HOLIDAY - THURSDAY, DECEMBER 24, 2020 THROUGH SUNDAY, DECEMBER 27, 2020
NEW YEAR'S HOLIDAY - THURSDAY, DECEMBER 31, 2020 THROUGH SUNDAY, JANUARY 3, 2020

FLAGGING OPERATION SHORT OR INTERMEDIATE TERM STATIONARY LANE CLOSURE ON 2-LANE 2-WAY ROADWAY

NOT TO SCALE

TABLE 6C-3 TAPER LENGTH CRITERIA FOR TEMPORARY TRAFFIC CONTROL ZONES. TABLE 6H-4 FORMULAS FOR DETERMINING TAPER LENGTHS. TABLE 6C-2 LONGITUDINAL BUFFER SPACE.

TABLE NY6H-3 ADVANCE WARNING SIGN SPACING. TABLE 6H-4 FORMULAS FOR DETERMINING TAPER LENGTHS (continued).



FLAGGING OPERATION SHORT OR INTERMEDIATE TERM STATIONARY LANE CLOSURE AT OR NEAR INTERSECTION

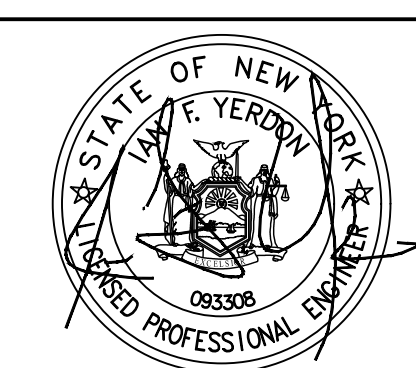
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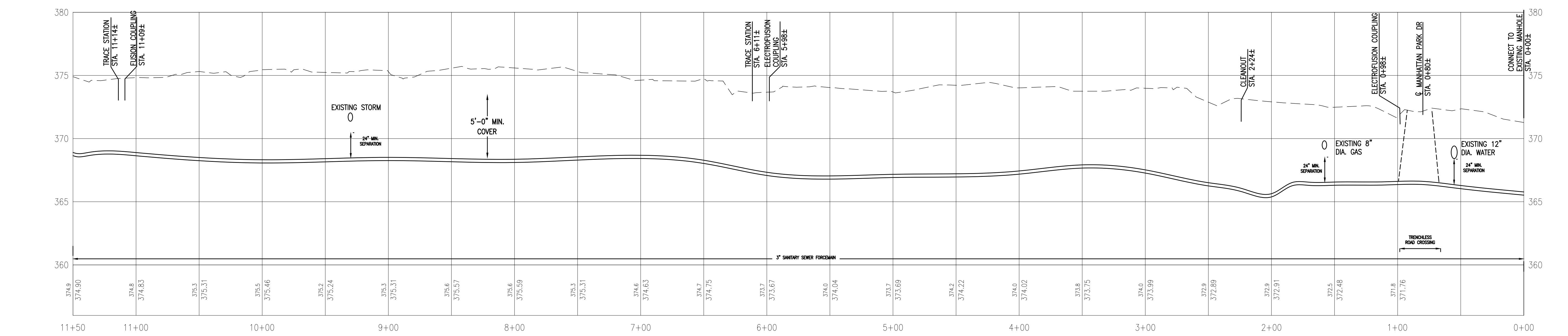
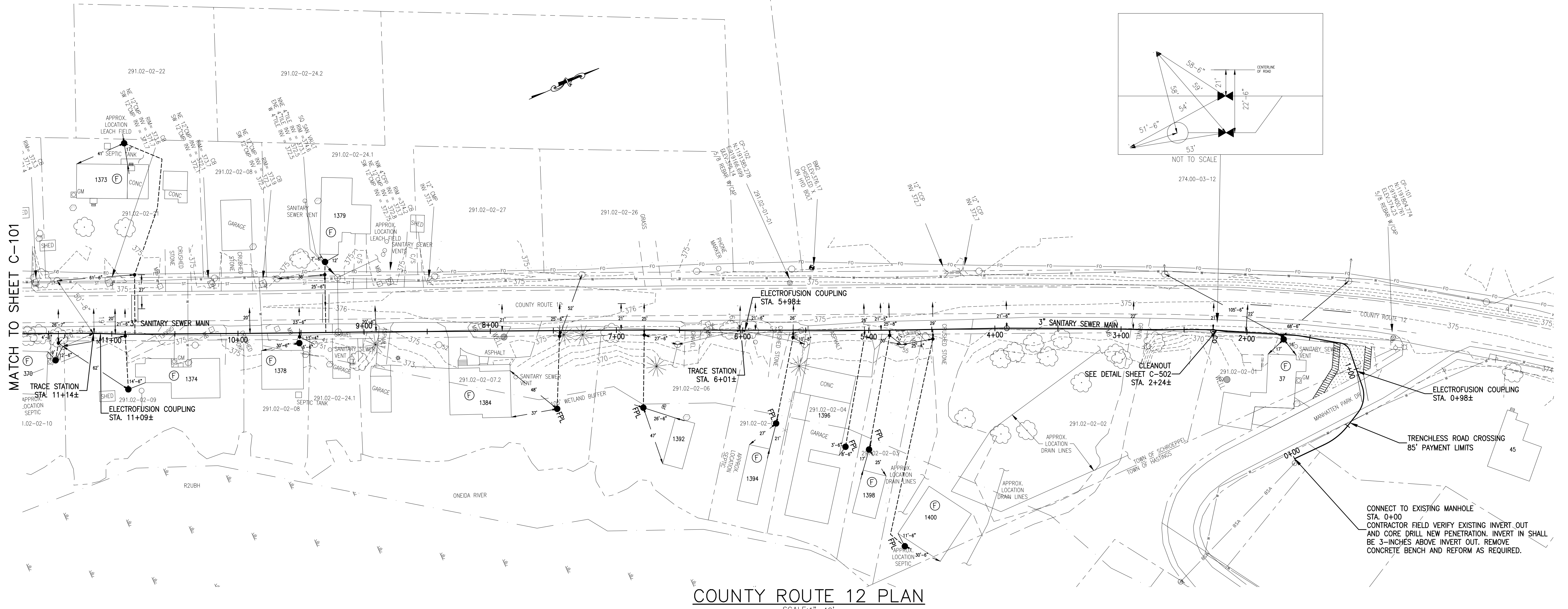
CALL DIG SAFELY NEW YORK BEFORE DIGGING @ 1-800-962-7962

SEE LEGEND & TABLES STANDARD SHEET (M619-11) FOR LEGEND OF SYMBOLS AND/OR LETTER CODES USED IN THIS DRAWING.

- NOTES:
1. AT SIGNALIZED INTERSECTIONS, SIGNALS SHALL BE TURNED OFF FOR ANY FLAGGING OPERATION, UNLESS OTHERWISE AUTHORIZED BY THE NYSOT.
2. WHEN PAVED SHOULDERS HAVING A WIDTH OF 8' OR MORE ARE CLOSED, CHANNELIZING DEVICES SHALL BE USED TO CLOSE THE SHOULDER IN ADVANCE TO DELINEATE THE BEGINNING OF THE WORK AREA AND DIRECT VEHICULAR TRAFFIC TO REMAIN IN THE TRAVEL WAY.
3. CHANNELIZING DEVICE SPACING (CENTER TO CENTER) SHALL NOT EXCEED 40' IN THE ACTIVE WORK SPACE.
4. THE END ROAD WORK SIGN (G20-2) SHALL BE PLACED A MAXIMUM OF 500' PAST THE END OF THE WORK SPACE.
5. WHERE DIRECTED BY THE ENGINEER, A BUFFER SPACE SHALL BE PROVIDED IN ORDER TO LOCATE THE ONE-LANE, TWO-WAY TRAFFIC TAPER PRIOR TO ANY HORIZONTAL OR VERTICAL CURVE, IN ORDER TO PROVIDE ADEQUATE SIGHT DISTANCE FOR THE FLAGGERS AND/OR A QUEUE OF STOPPED VEHICLES.
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9. ALL FLAGGERS SHALL USE 24" (MIN) OCTAGON SHAPED STOP/SLOW PADDLES HAVING 6" STAFF.
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11. CENTERLINE CHANNELIZING DEVICES ARE OPTIONAL AND MAY BE ELIMINATED WHERE SPACE CONSTRAINTS EXIST.
12. NO WORK ACTIVITY, EQUIPMENT, OR STORAGE OF VEHICLE, OR MATERIAL SHALL OCCUR WITHIN THE BUFFER SPACE AT ANY TIME.
13. FOR PEDESTRIAN DETOUR ACCOMMODATIONS REFER TO THE NYSOT STANDARD SHEETS TITLED "SIDEWALK DETOUR OR DIVERSION" AND "CROSSWALK CLOSURE AND PEDESTRIAN DETOUR".

DESIGNED BY: SARINO, JACOB, 180022.23.04
CHECKED BY: SARINO, JACOB, 180022.23.04
APPROVED BY: SARINO, JACOB, 180022.23.04
PLOTTED BY: SARINO, JACOB, 180022.23.04





NOTE:  
THE PROPERTY LINES SHOWN ON THIS DRAWING WERE OBTAINED FROM COUNTY TAX MAPS. THEIR LOCATION MAY NOT BE ENTIRELY TRUE AND CORRECT AND ARE NOT INTENDED FOR USE IN THE CONVEYANCE OF LAND. THE ACTUAL LOCATION OF THESE PROPERTY LINES IS SUBJECT TO SUCH VARIATIONS AND CORRECTIONS AS MIGHT RESULT FROM AN ACCURATE INSTRUMENT SURVEY.

THE LOCATION, SIZES AND ELEVATIONS OF THE EXISTING UTILITIES SHOWN ON THIS DRAWING WERE OBTAINED FROM AVAILABLE DRAWINGS AND EVIDENCE OF ABOVE GROUND FEATURES. THEIR LOCATION, SIZE AND ELEVATION MAY NOT BE ENTIRELY TRUE AND CORRECT. OTHER UTILITIES MAY EXIST. ALL LOCAL UTILITY COMPANIES SHOULD BE NOTIFIED BEFORE EXCAVATION.

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C2AE



WASTEWATER COLLECTION SYSTEM  
AINSLIEE DRIVE SEWER DISTRICT  
OSWEGO COUNTY, NY

COUNTY ROUTE 12 - PLAN & PROFILE

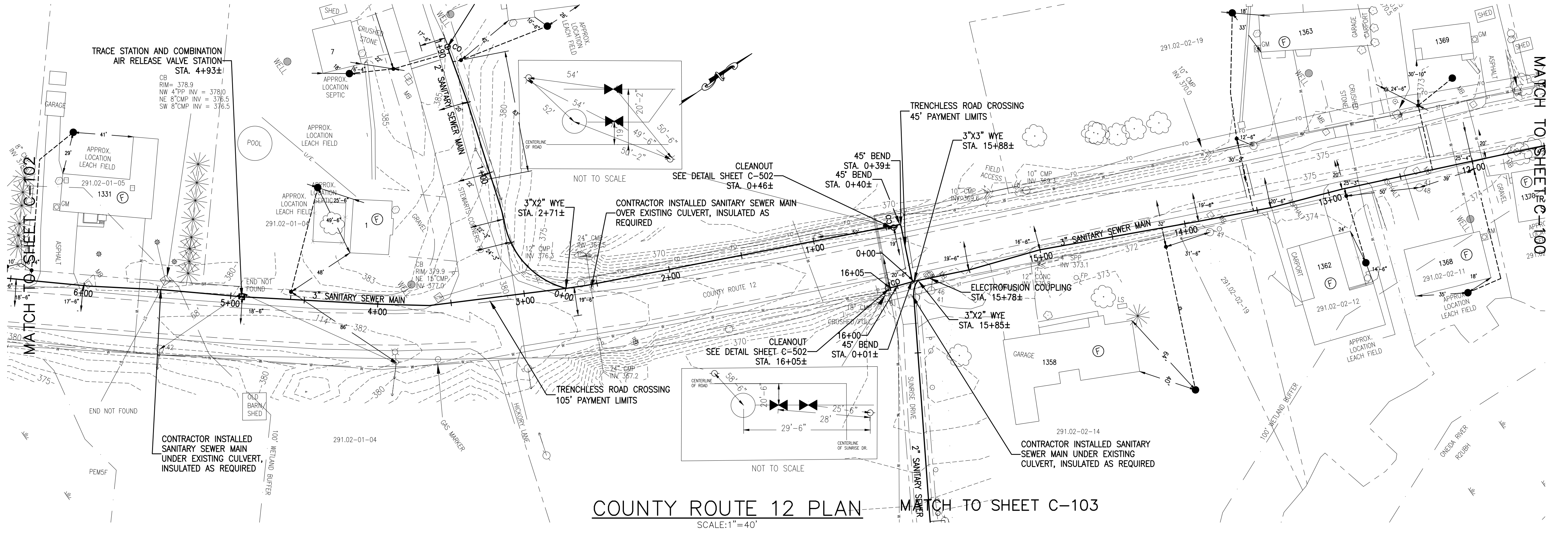
REV	DESCRIPTION	DATE
FOR RECORD		MARCH 2021

PROJ. #: 170003  
DATE: AUGUST 2021  
SHEET: C-100

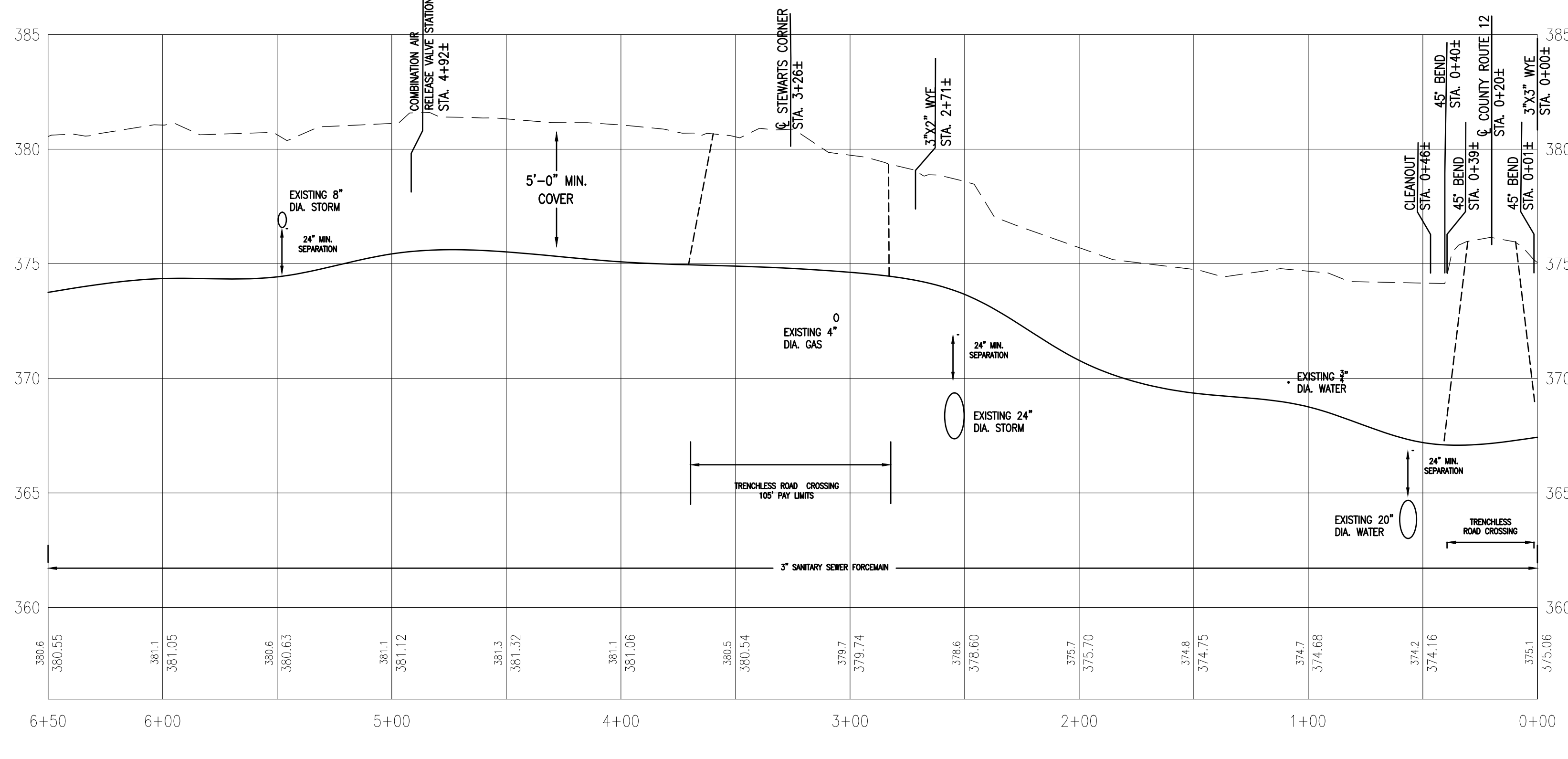
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CHECKED BY: [Name]  
APPROVED BY: [Name]



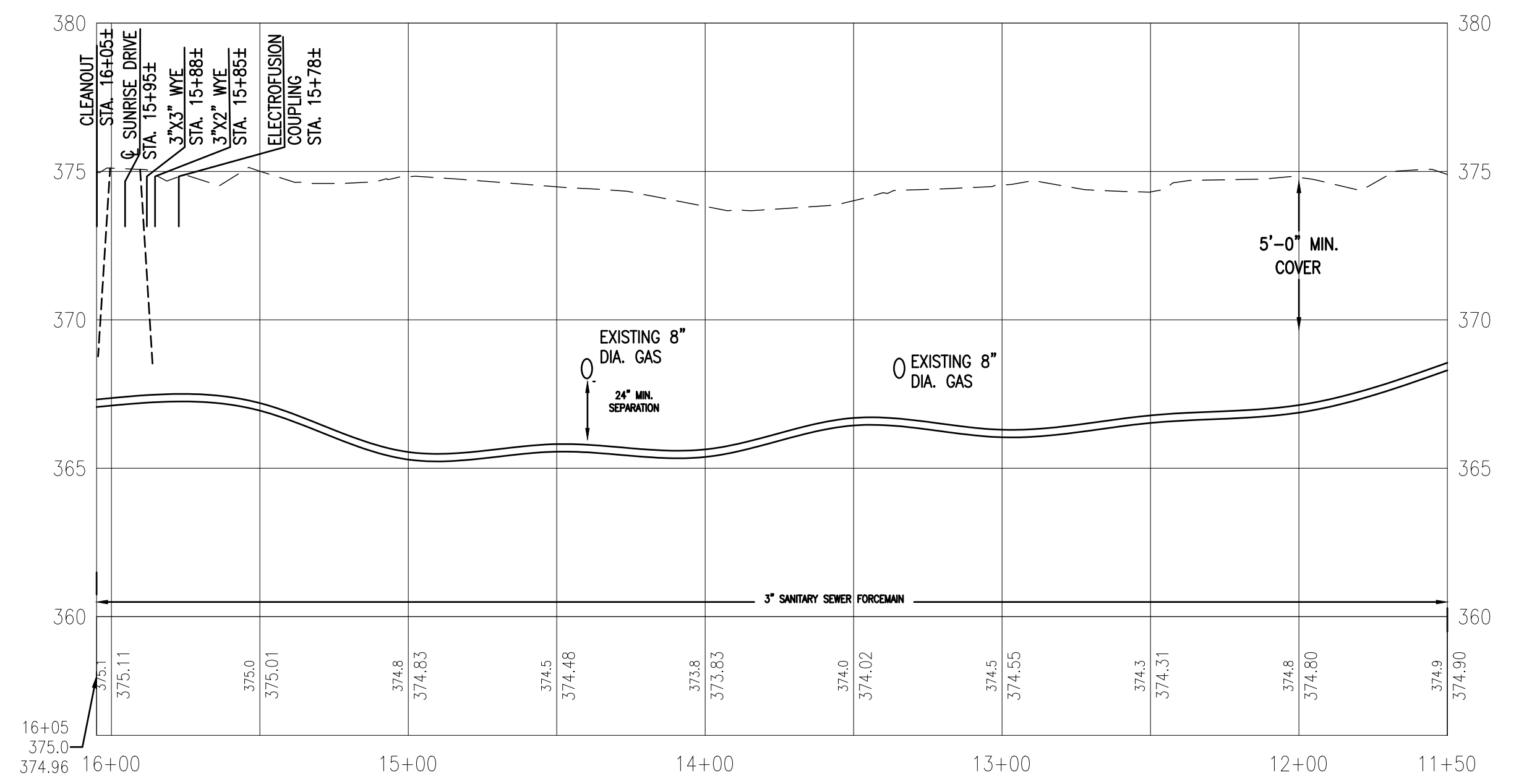
MATCH TO SHEET C-103



COUNTY ROUTE 12 PLAN - MATCH TO SHEET C-103  
SCALE: 1" = 40'



COUNTY ROUTE 12 PROFILE  
SCALE: 1" = 40' HORIZ.  
SCALE: 1" = 4' VERT.



COUNTY ROUTE 12 PROFILE  
SCALE: 1" = 40' HORIZ.  
SCALE: 1" = 4' VERT.

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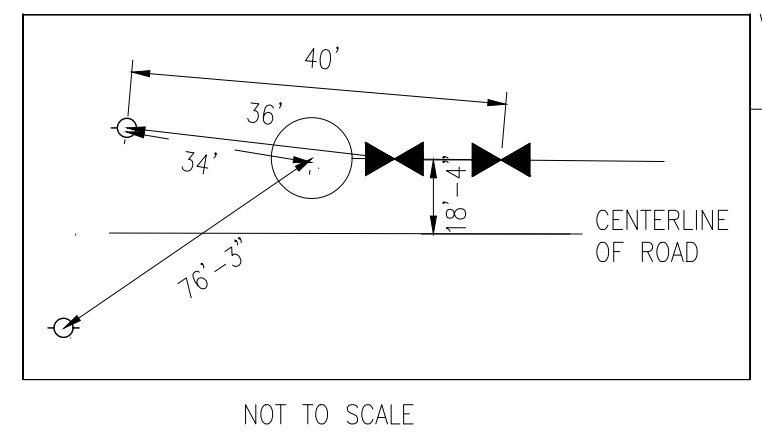
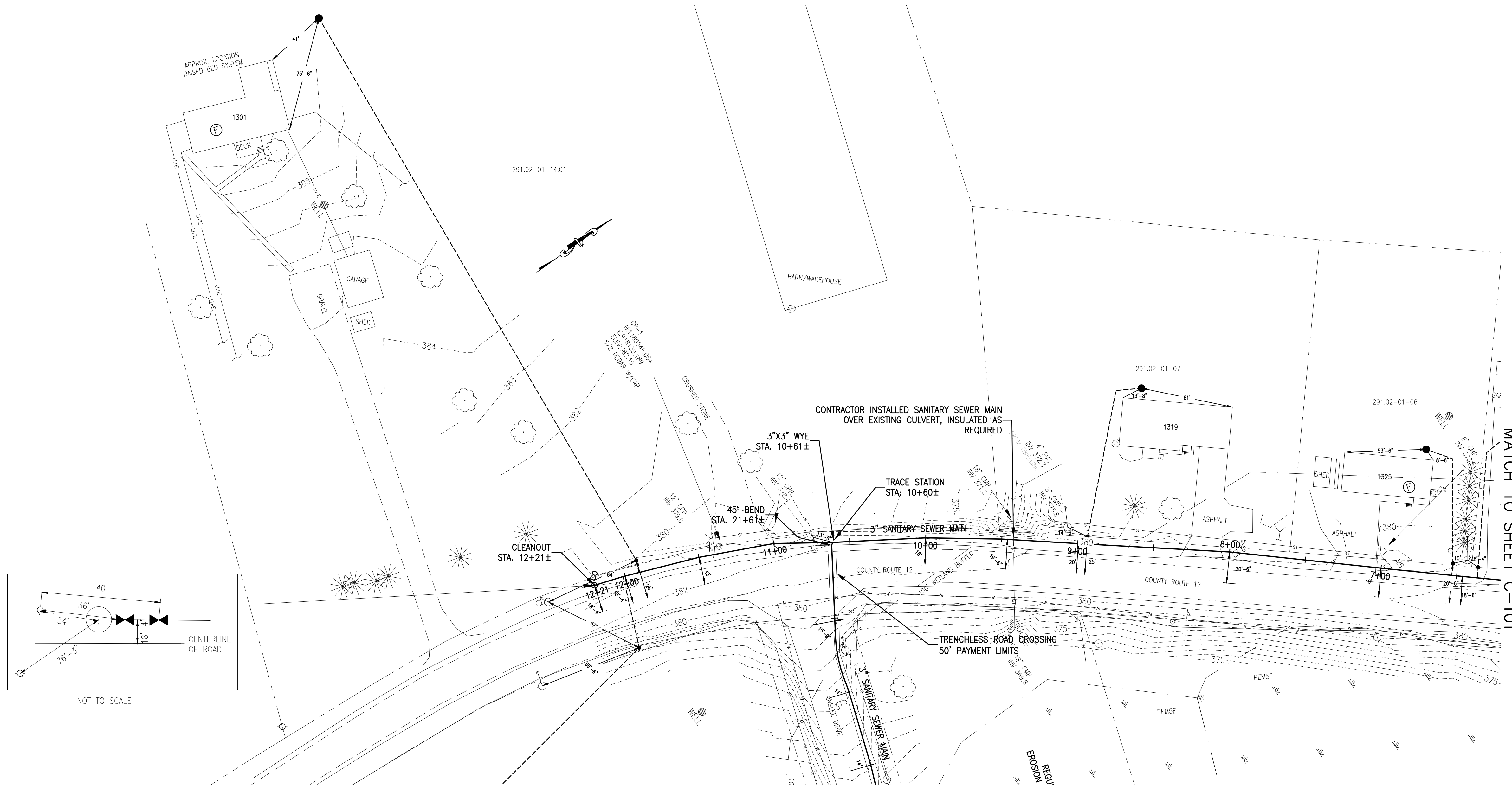
REVISIONS		
REV	DESCRIPTION	DATE

DESIGNED BY: SAUNDERS, ACOB, 8/20/21 2:24 PM  
CHECKED BY: SAUNDERS, ACOB, 8/20/21 2:24 PM  
DATE: 8/20/21  
PROJECT: WASTEWATER COLLECTION SYSTEM - AINSLEE DRIVE SEWER DISTRICT  
SHEET: C-101

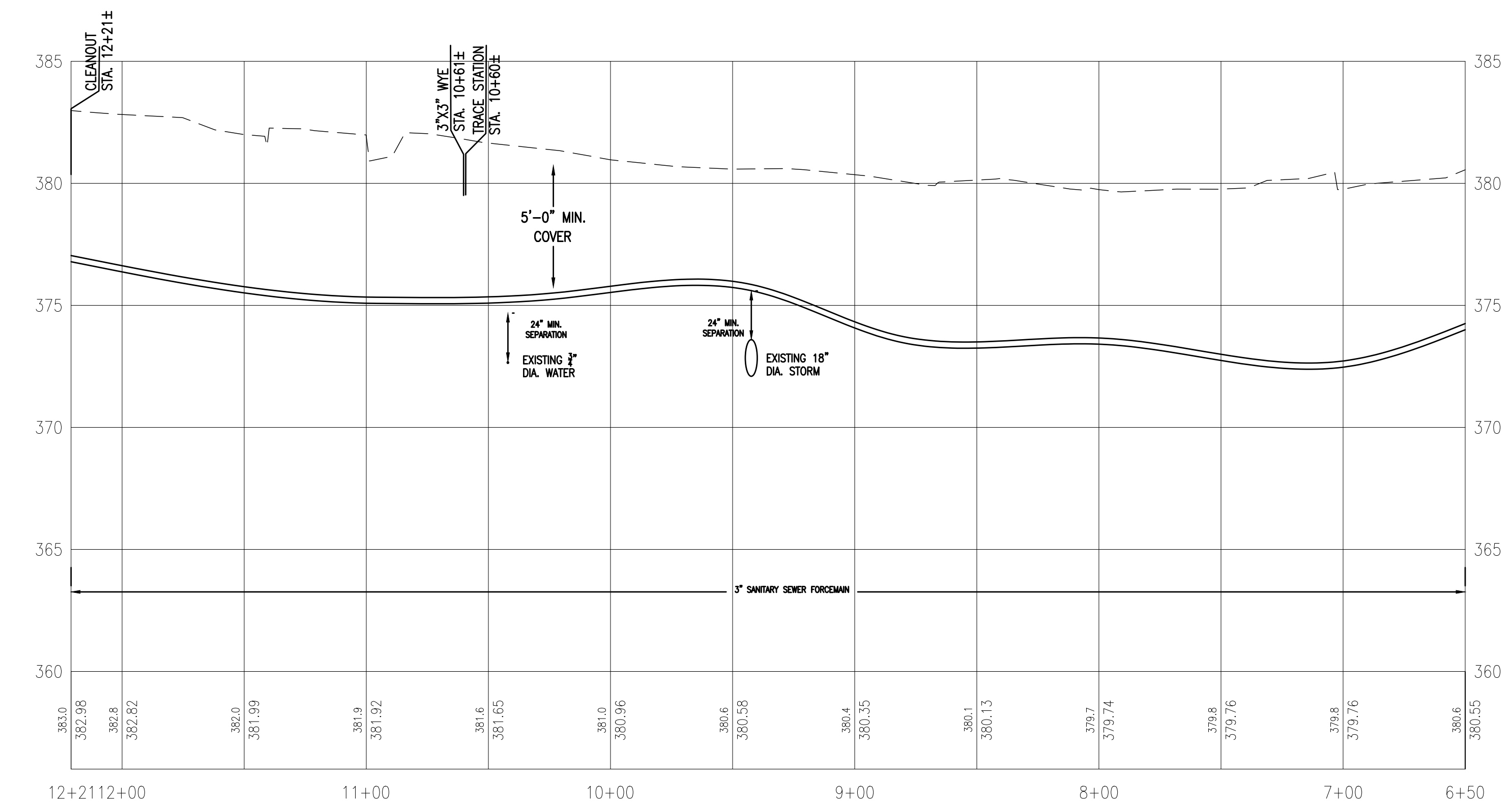


REVISIONS		
REV	DESCRIPTION	DATE
FOR RECORDED		
	MARCH 2021	

PROJ. #: 17003  
DATE: AUGUST 2021  
SHEET



COUNTY ROUTE 12 PLAN MATCH TO SHEET C-104  
SCALE: 1" = 40'



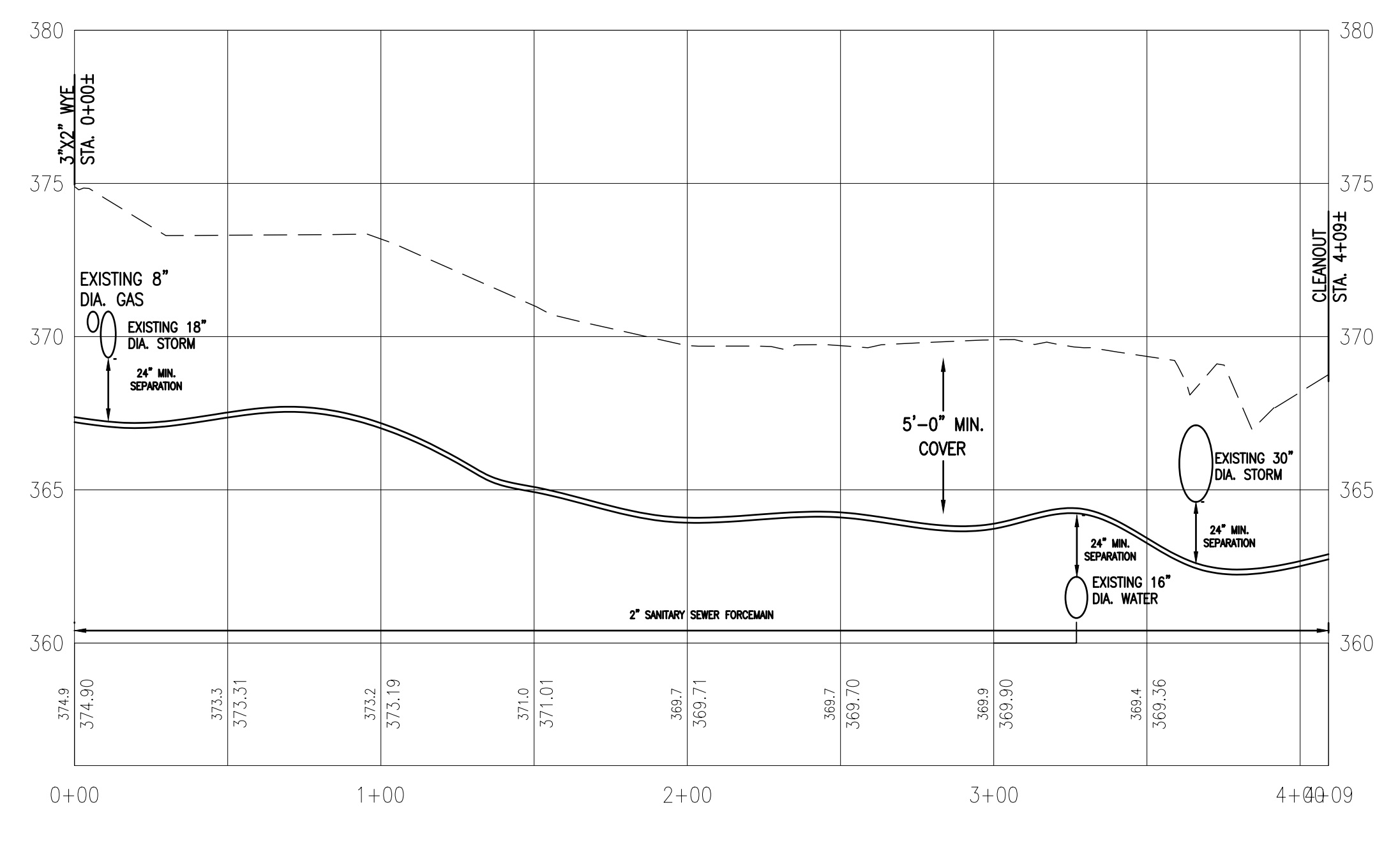
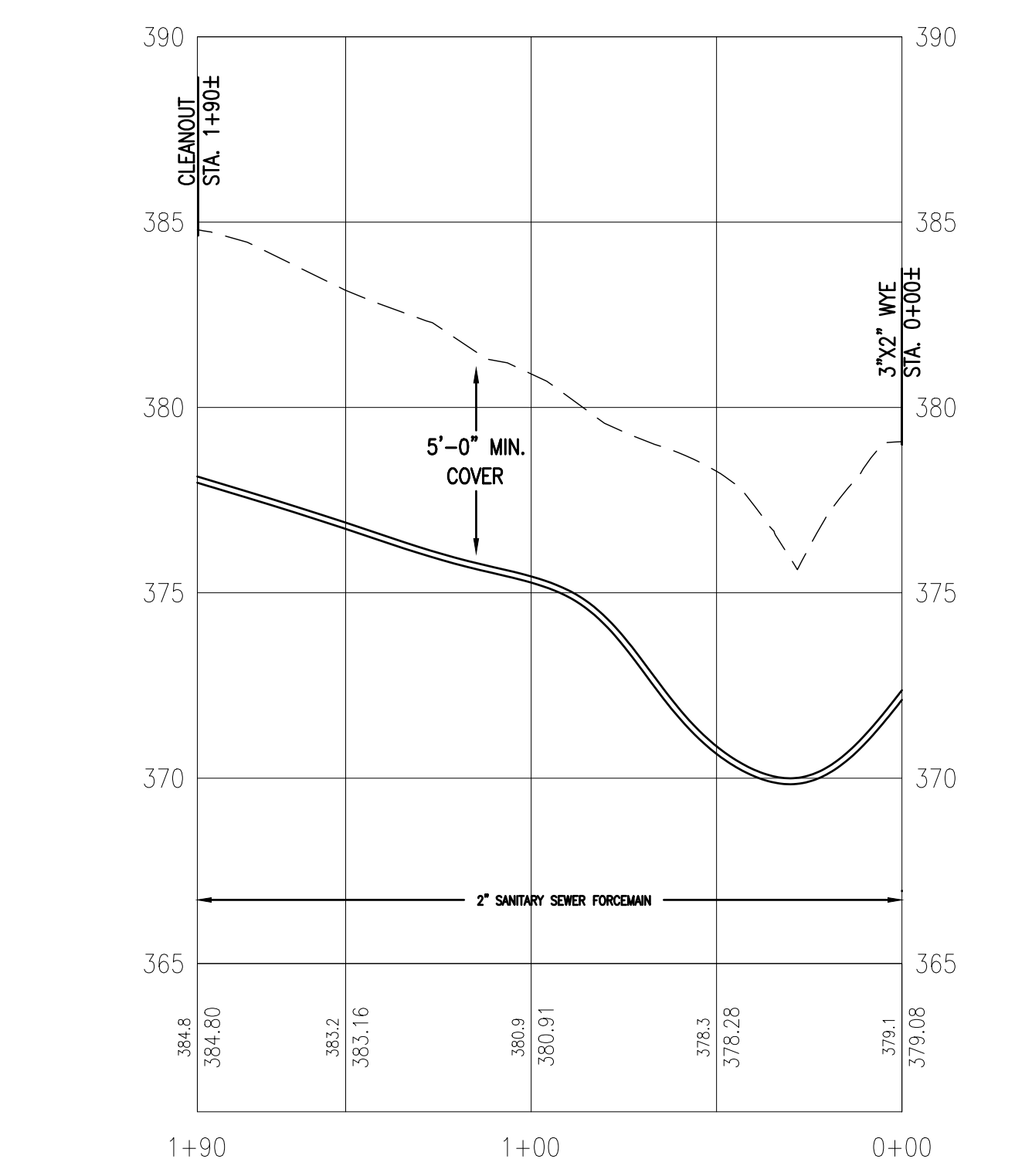
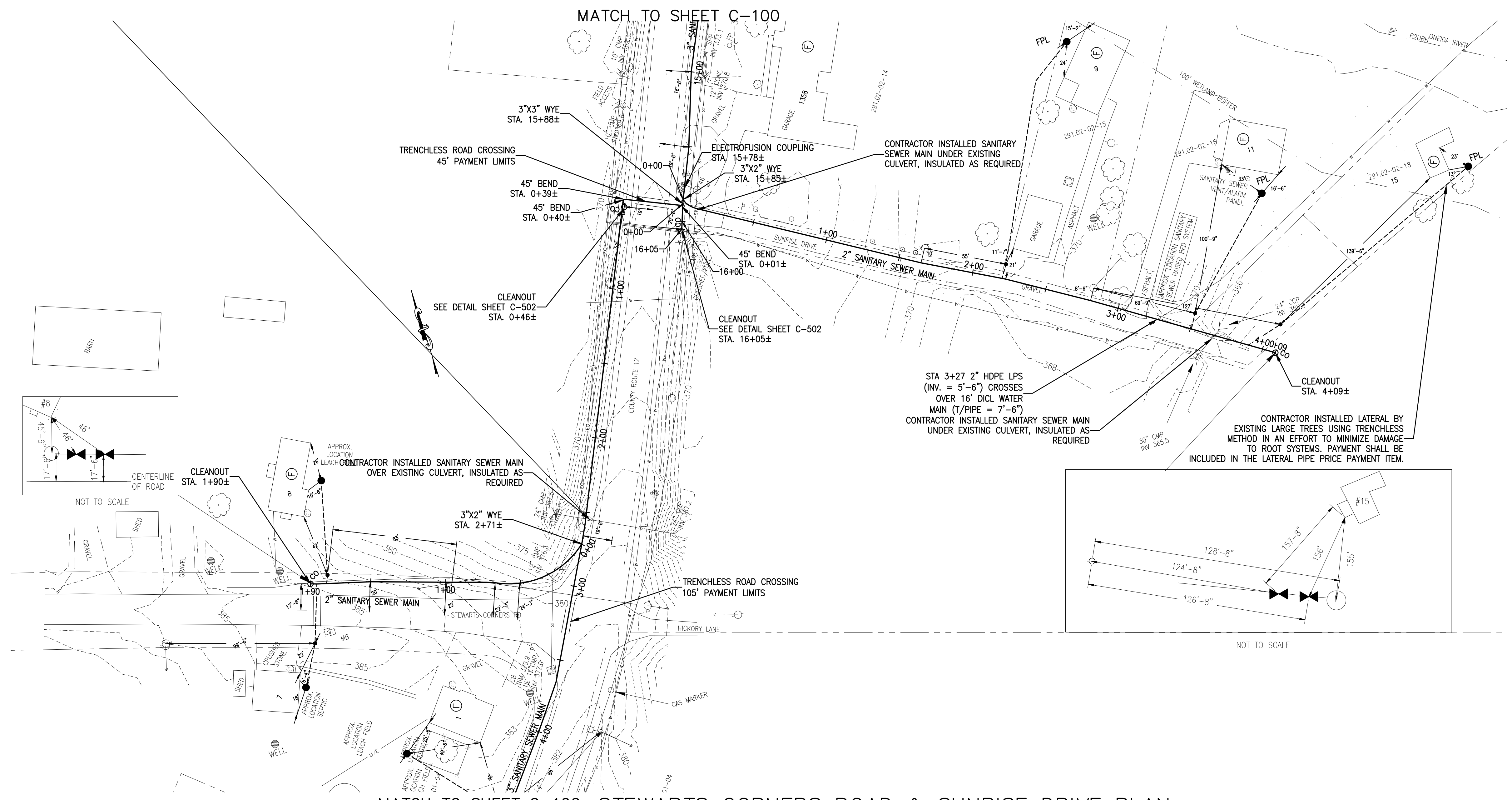
COUNTY ROUTE 12 PROFILE  
SCALE: 1" = 40' HORIZ.  
SCALE: 1" = 4' VERT.

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C2AE



DESIGNED BY: [Redacted]  
CHECKED BY: [Redacted]  
DATE: [Redacted]  
PROJECT: [Redacted]

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THE LOCATION, SIZES AND ELEVATIONS OF THE EXISTING UTILITIES SHOWN ON THIS DRAWING WERE OBTAINED FROM AVAILABLE DRAWINGS AND EVIDENCE OF ABOVE GROUND FEATURES. THEIR LOCATION, SIZE AND ELEVATION MAY NOT BE ENTIRELY TRUE AND CORRECT. OTHER UTILITIES MAY EXIST. ALL LOCAL UTILITY COMPANIES SHOULD BE NOTIFIED BEFORE EXCAVATION.



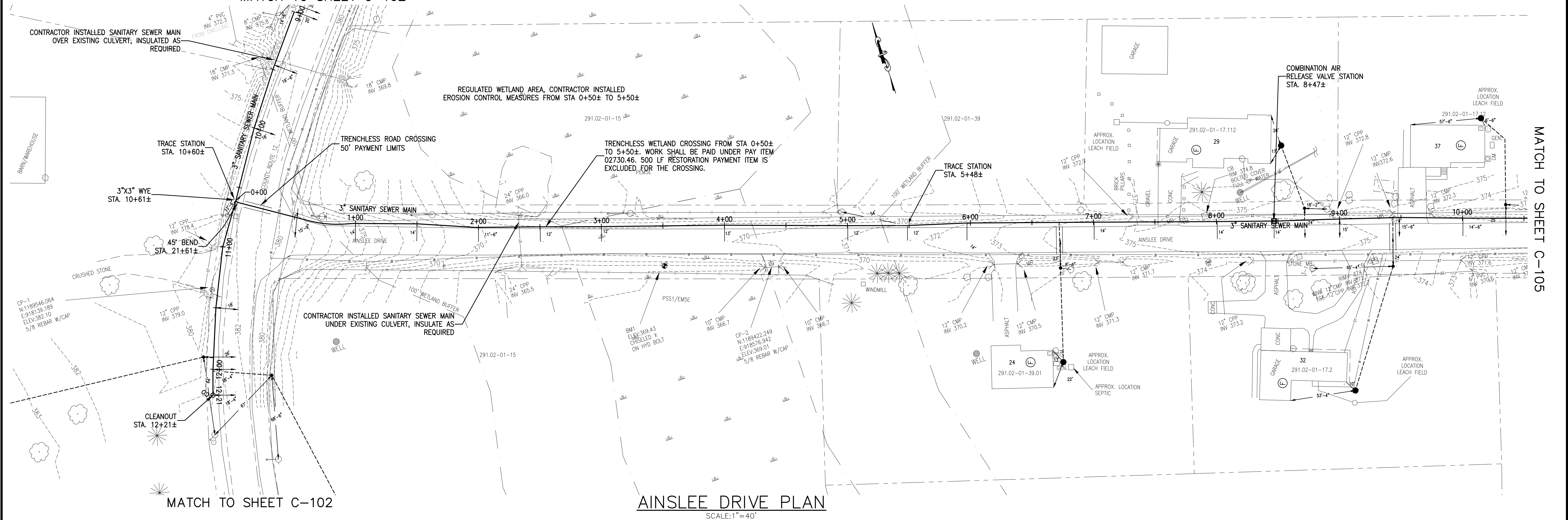
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C2AE



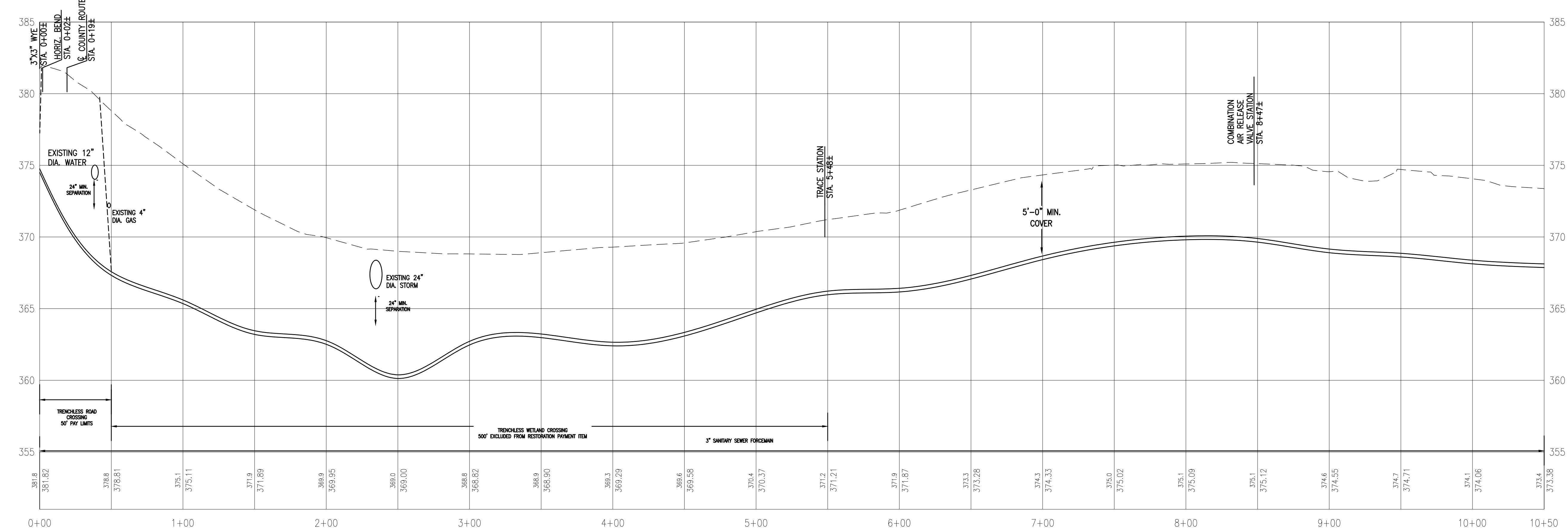


MATCH TO SHEET C-102



MATCH TO SHEET C-102

AINSLEE DRIVE PLAN  
SCALE: 1" = 40'



AINSLEE DRIVE PROFILE  
SCALE: 1" = 40' HORIZ.  
SCALE: 1" = 4' VERT.

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WASTEWATER COLLECTION SYSTEM  
AINSLEE DRIVE SEWER DISTRICT  
OSSEGO COUNTY, NY

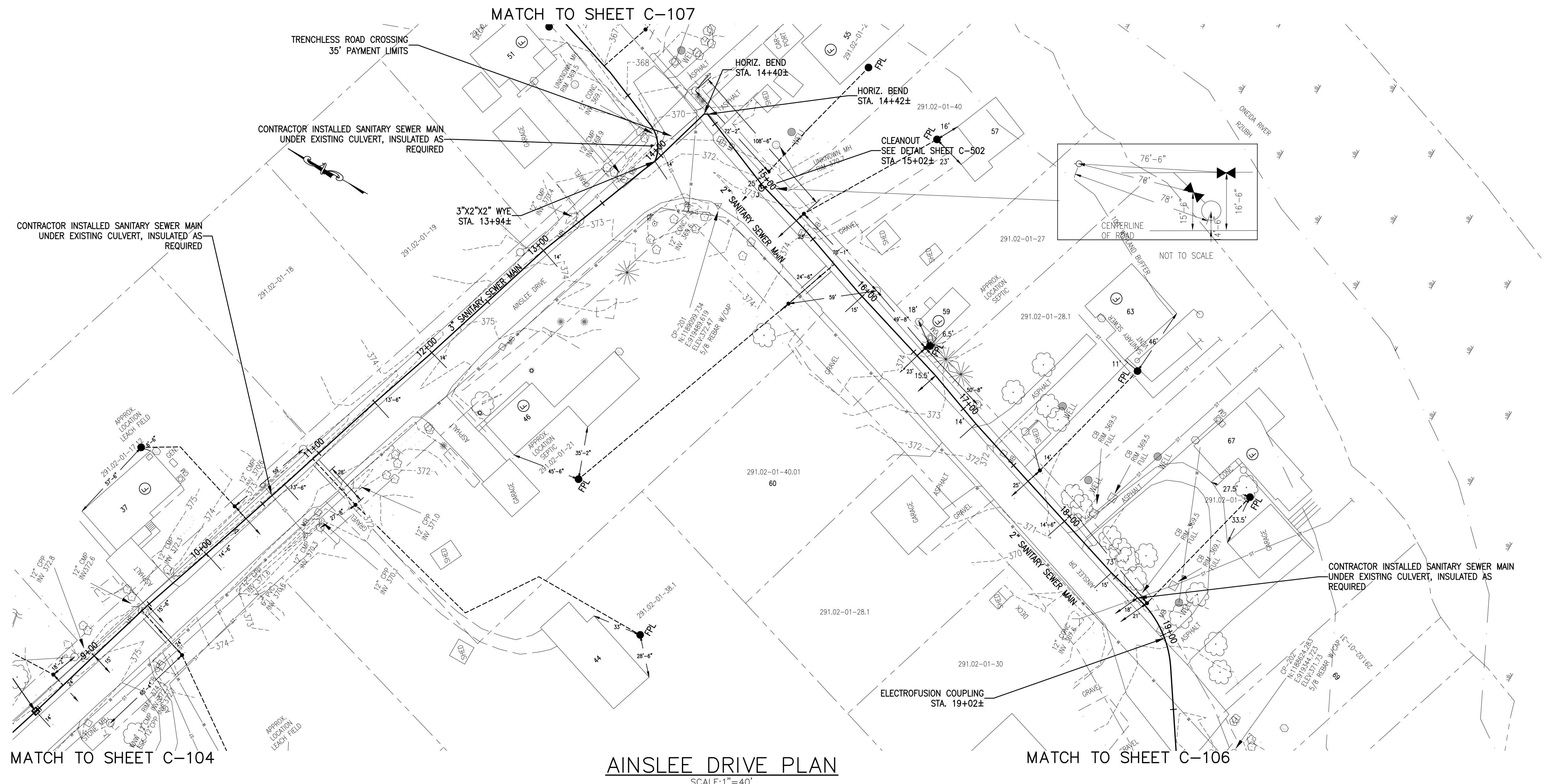
AINSLEE DRIVE - PLAN & PROFILE

REVISIONS		
REV	DESCRIPTION	DATE
FOR RECORD		
MARCH 2021		

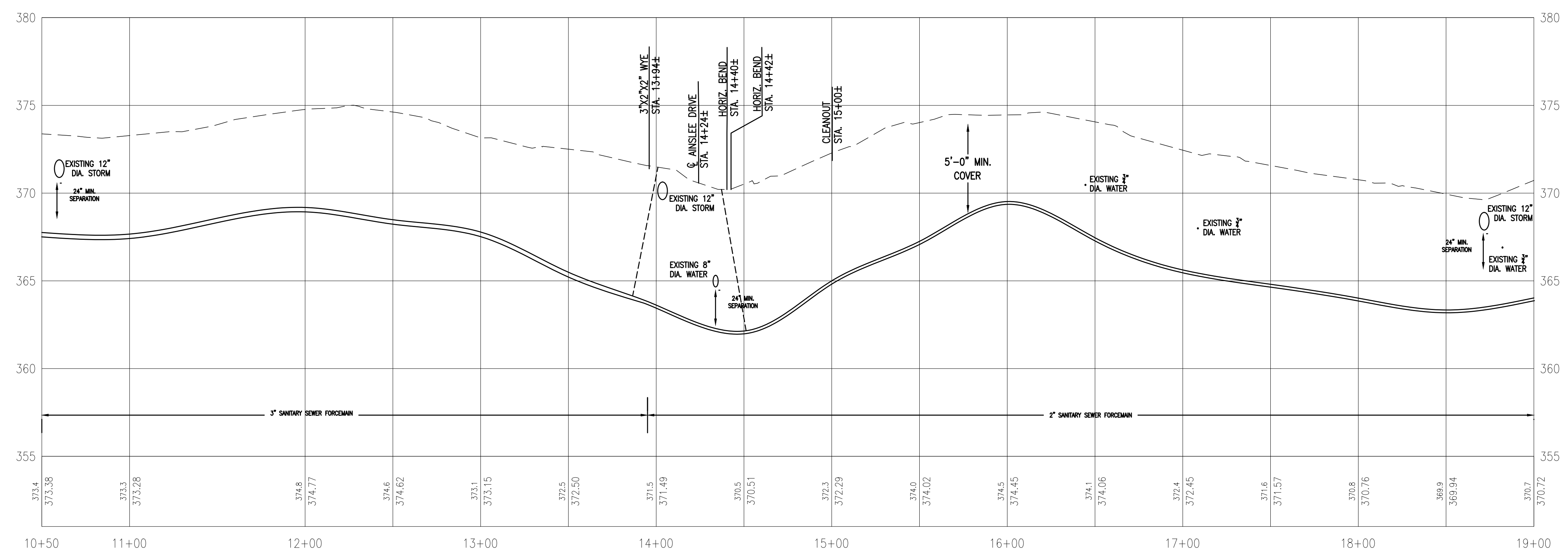
PROJ. #: 17003  
DATE: AUGUST 2021  
SHEET  
C-104

70 MAIN STREET, PO BOX 368, CANTON, NY 13617





AINSLIEE DRIVE PLAN  
SCALE: 1" = 40'



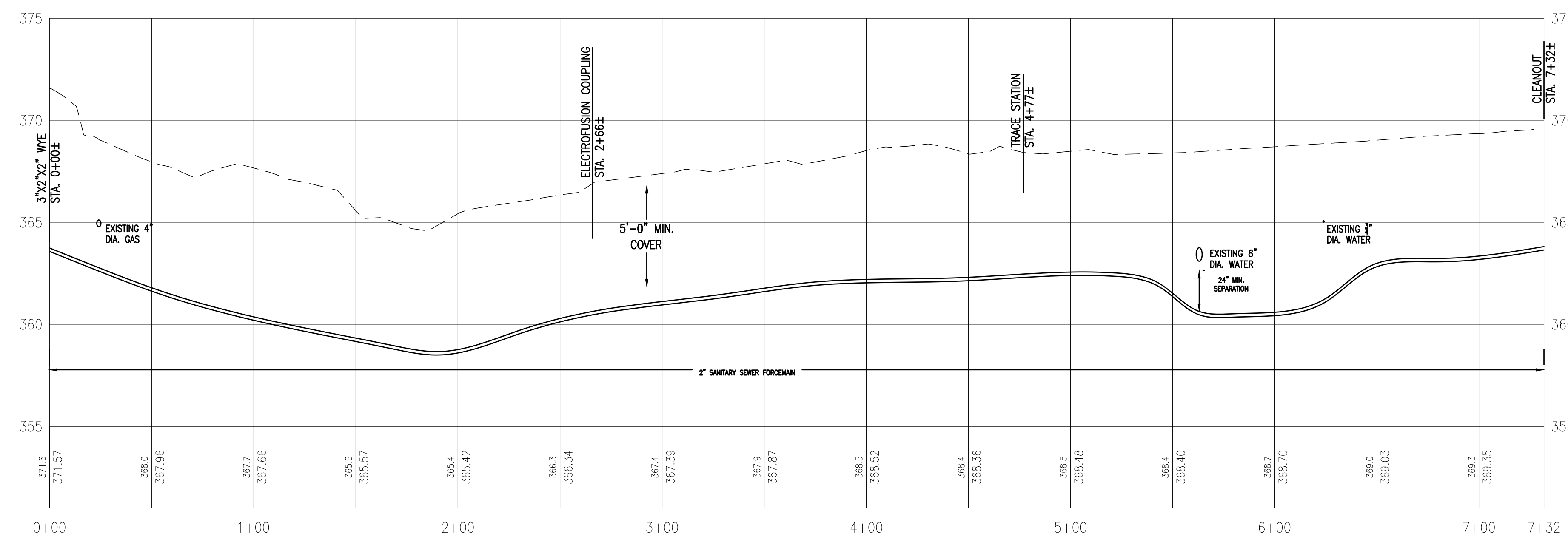
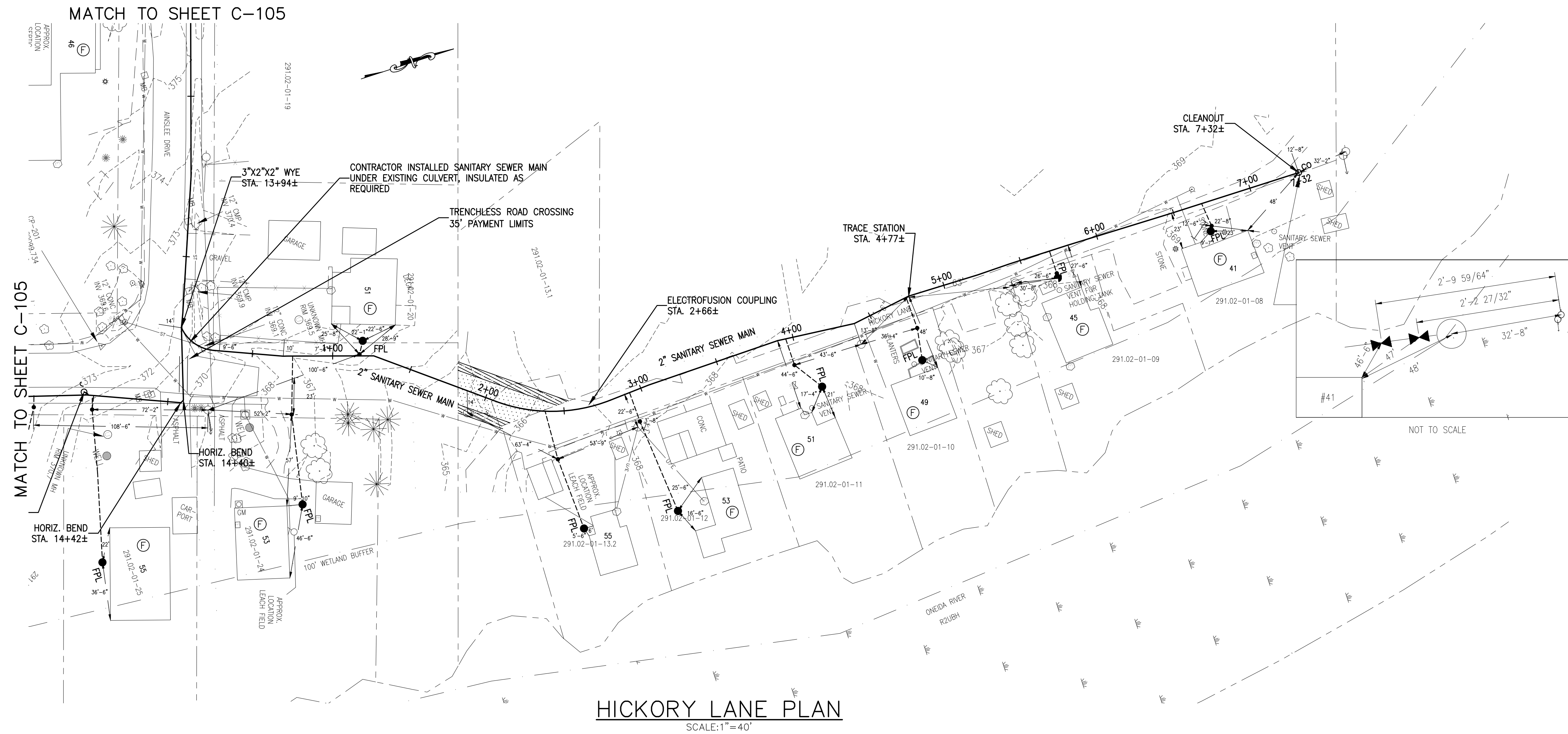
AINSLIEE DRIVE PROFILE  
SCALE: 1" = 40' HORIZ.  
SCALE: 1" = 4' VERT.

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C2AE







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C2AE



70 MAIN STREET, PO BOX 368, CANTON, NY 13617

REVISIONS

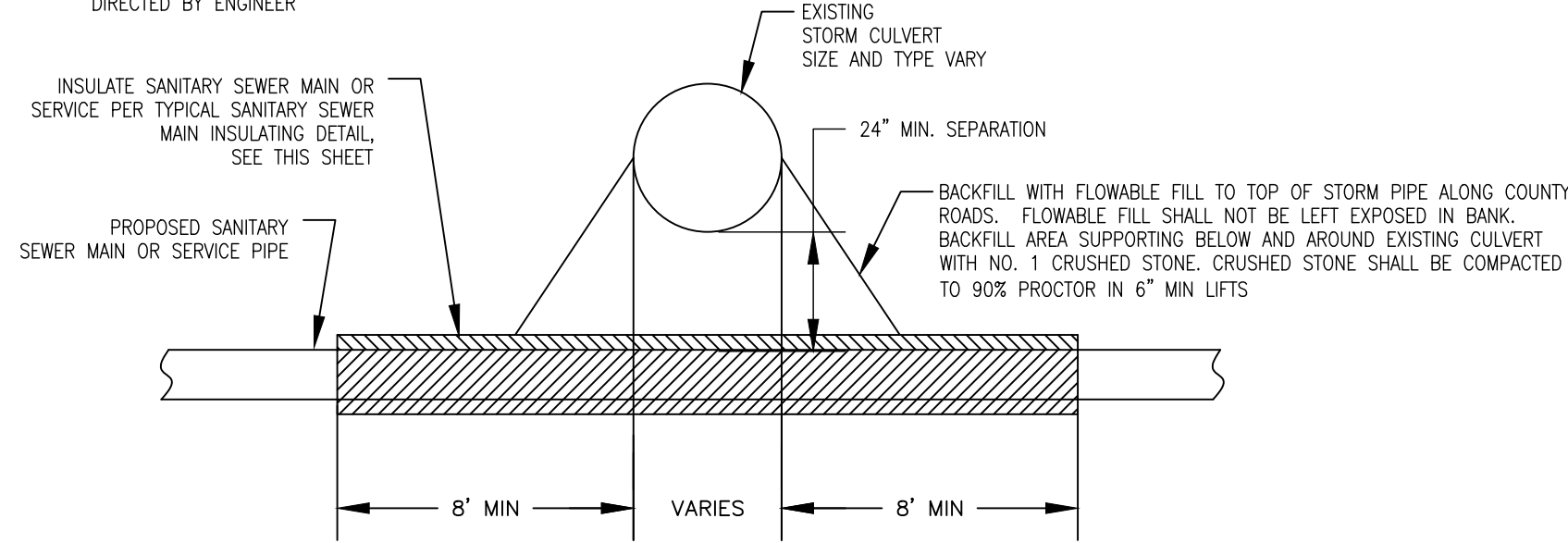
REV	DESCRIPTION	DATE
FOR RECORD		MARCH 2021

PROJ. #: 170003  
DATE: AUGUST 2021  
SHEET



WE RECYCLE

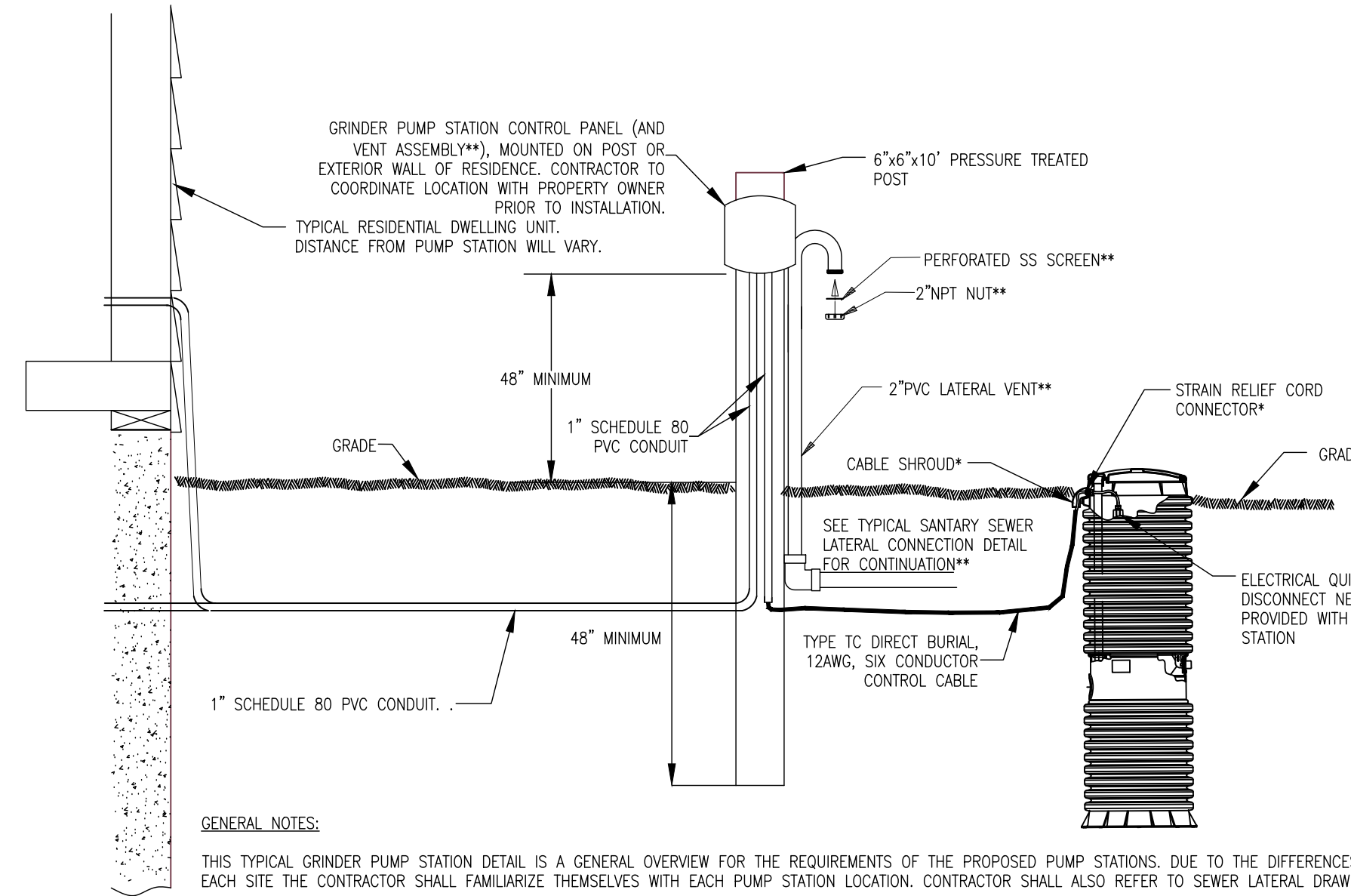
NOTE: USE INSULATION WHENEVER SANITARY SEWER MAIN OR SERVICE PIPING WILL PASS WITHIN TWO FEET OF A CULVERT PIPE OR IF DIRECTED BY ENGINEER



NOTE: ANY AND ALL SILT FENCE OR EROSION CONTROL MEASURES AT CULVERT CROSSING AREAS SHALL BE INSTALLED IMMEDIATELY (OR WITHIN THE SAME DAY) OF THE PROPOSED UTILITY BEING INSTALLED IN THE CULVERT AREA. SEE DETAIL "EROSION CONTROL AT CULVERT ENDS" ON DETAIL SHEET C-504 FOR DETAILS.

### TYPICAL CULVERT CROSSING

N.T.S.



GENERAL NOTES:

THIS TYPICAL GRINDER PUMP STATION DETAIL IS A GENERAL OVERVIEW FOR THE REQUIREMENTS OF THE PROPOSED PUMP STATIONS. DUE TO THE DIFFERENCES AT EACH SITE THE CONTRACTOR SHALL FAMILIARIZE THEMSELVES WITH EACH PUMP STATION LOCATION. CONTRACTOR SHALL ALSO REFER TO SEWER LATERAL DRAWINGS.

ALL CONDUIT PENETRATIONS SHALL BE THROUGH THE BOTTOM OF THE PUMP CONTROL PANEL. NO PENETRATIONS SHALL BE MADE THROUGH THE TOP OR SIDES. ALL CONDUIT PENETRATIONS SHALL BE WATER TIGHT AND SEALED TO AVOID MOISTURE AND MOISTURE VAPOR FROM ENTERING PANEL.

ALL CONDUIT SHALL BE MINIMUM 1" SCHEDULE 80 PVC AND ALL WIRING SHALL BE RATED FOR THE ENVIRONMENT IN WHICH IT IS INSTALLED. BURIED CONDUIT SHALL BE INSTALLED A MINIMUM 24" BELOW GRADE EXCEPT WHERE A DEEPER DEPTH IS REQUIRED BY NEC ARTICLE 300, TABLE 300-5. ALL WIRES SHALL BE MINIMUM #12 AWG. BRANCH CIRCUIT SIZES SHALL BE ADJUSTED FOR VOLTAGE DROP PER NEC REQUIREMENTS.

CONDUIT RUN AND BRANCH CIRCUIT WIRING LENGTHS SHALL VARY FOR EACH INSTALLATION. REFER TO DRAWING PLANS AND COORDINATE INSTALLATION WITH PROPERTY OWNERS AND ENGINEER.

ALL BRANCH CIRCUITS SHALL BE TERMINATED AT RESIDENCE'S ELECTRICAL PANEL WITH A 30 AMP, 1-POLE CIRCUIT BREAKER.

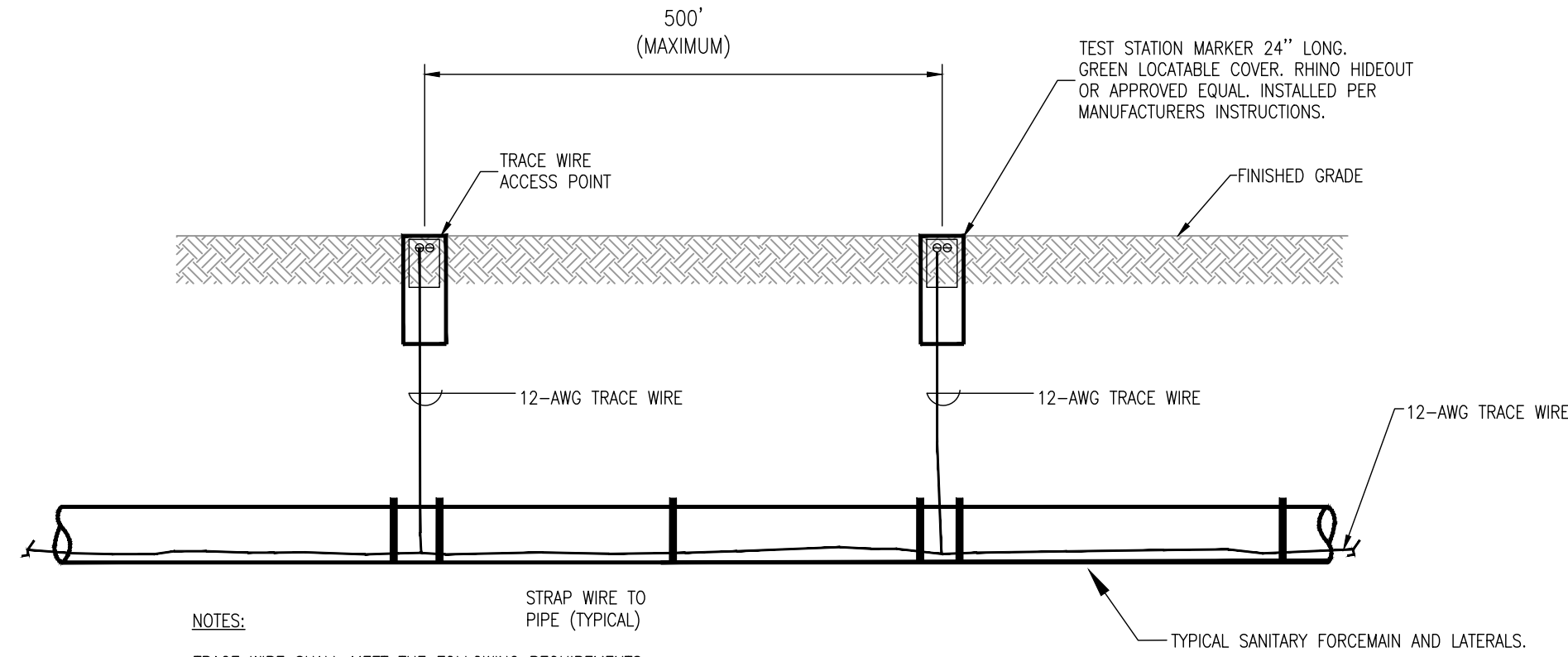
REFER TO TRENCH DETAIL, FOR ADDITIONAL BURIED CONDUIT REQUIREMENTS.

\*SEE TYPICAL SANITARY SEWER LATERAL CONNECTION FOR ILLUSTRATION OF FPL CONDUIT CONNECTION

\*\*VENT ASSEMBLY ONLY REQUIRED ON FPL GRINDER INSTALLATIONS

### TYPICAL ELECTRICAL POWER DETAIL

N.T.S.



NOTES:

TRACE WIRE SHALL MEET THE FOLLOWING REQUIREMENTS:

TRACE WIRE SHALL BE COPPERHEAD COPPER-CLAD STEEL 12-AWG HIGH STRENGTH, HIGH CARBON WITH MINIMUM 450 LB. BREAK LOAD, MIN. 30 MIL HDPE INSULATION THICKNESS, OR APPROVED EQUAL.

TRACE WIRE SHALL BE COPPERHEAD COPPER-CLAD STEEL 12-AWG EXTRA HIGH STRENGTH WITH MINIMUM 1,150 LB. BREAK LOAD, MIN. 45 MIL HDPE INSULATION THICKNESS, OR APPROVED EQUAL.

ALL TRACE WIRE SHALL BE INTERCONNECTED AT INTERSECTIONS WITH A SINGLE, THREE-WAY LOCKING CONNECTOR, COPPERHEAD SNAKEBITE OR APPROVED EQUAL.

CONTRACTOR SHALL STRAP TRACE WIRE TO THE BOTTOM HALF OF THE PIPE AND SECURED (TAPED/TIED) AT 5-FOOT INTERVALS.

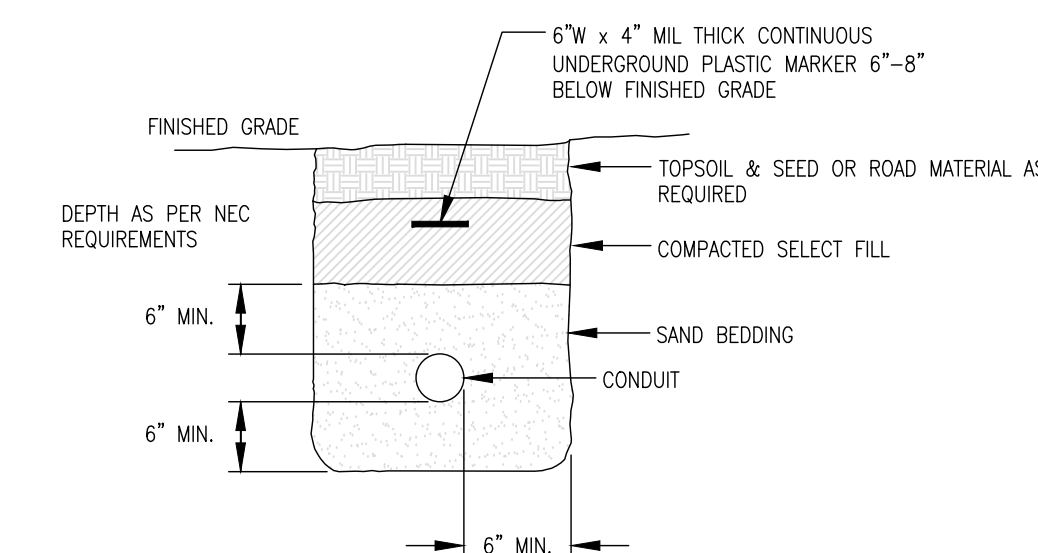
TRACE WIRE SHALL TERMINATE AT AN APPROVED TRACER WIRE ACCESS POINT COLOR CODED GREEN AND LOCATED DIRECTLY ABOVE THE FOREMAN EVERY 500' OR LESS. APPROVED TRACER WIRE ACCESS POINT SHALL ALSO BE USED AT LATERAL AND FOREMAN TERMINATION POINTS.

CONTRACTOR SHALL TEST WIRE BETWEEN ACCESS POINTS USING TYPICAL LOW FREQUENCY LINE TRACING EQUIPMENT, WITNESSED BY THE CONTRACTOR, ENGINEER, AND FACILITY OWNER AS APPLICABLE. CONTINUITY TESTING IN LIEU OF ACTUAL LINE TRACING SHALL NOT BE ACCEPTED.

CONTRACTOR SHALL COORDINATE WITH PIPING MANUFACTURER THE EXACT REQUIREMENTS FOR TRACE WIRE INSTALLATION. FACTORS SUCH AS PIPE EXPANSION INFLUENCE THE PROPER METHOD FOR INSTALLING TRACE WIRE. REFER TO SPECIFICATION \*02730 - SANITARY SEWER SYSTEM\* FOR ADDITIONAL REQUIREMENTS.

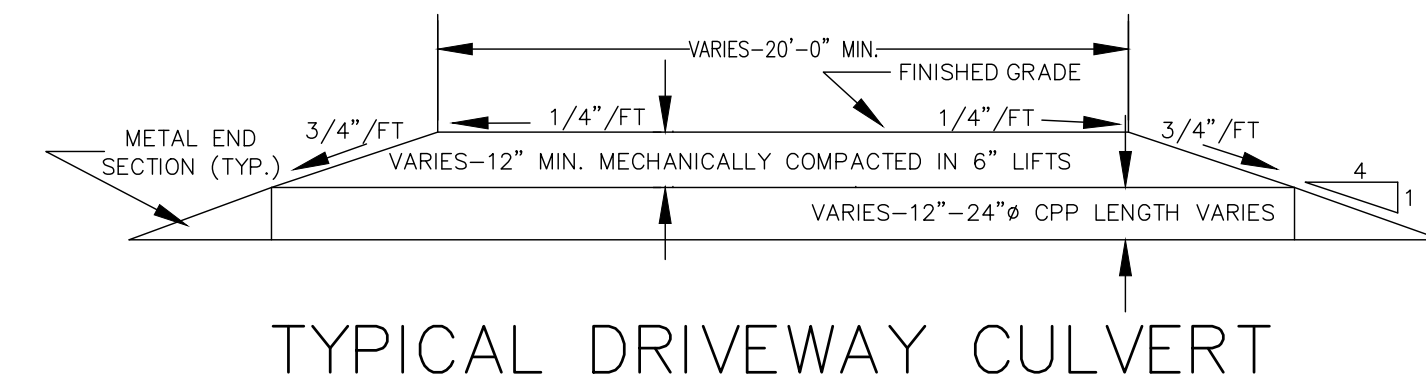
### TYPICAL SANITARY SEWER COLLECTION PIPING TRACE WIRE

N.T.S.



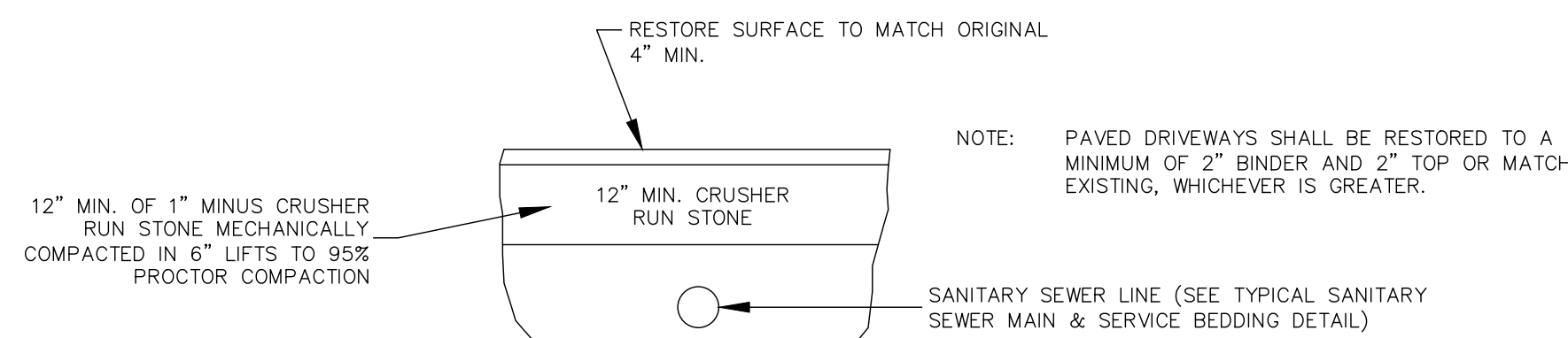
### TYPICAL ELECTRICAL TRENCH DETAIL

N.T.S.



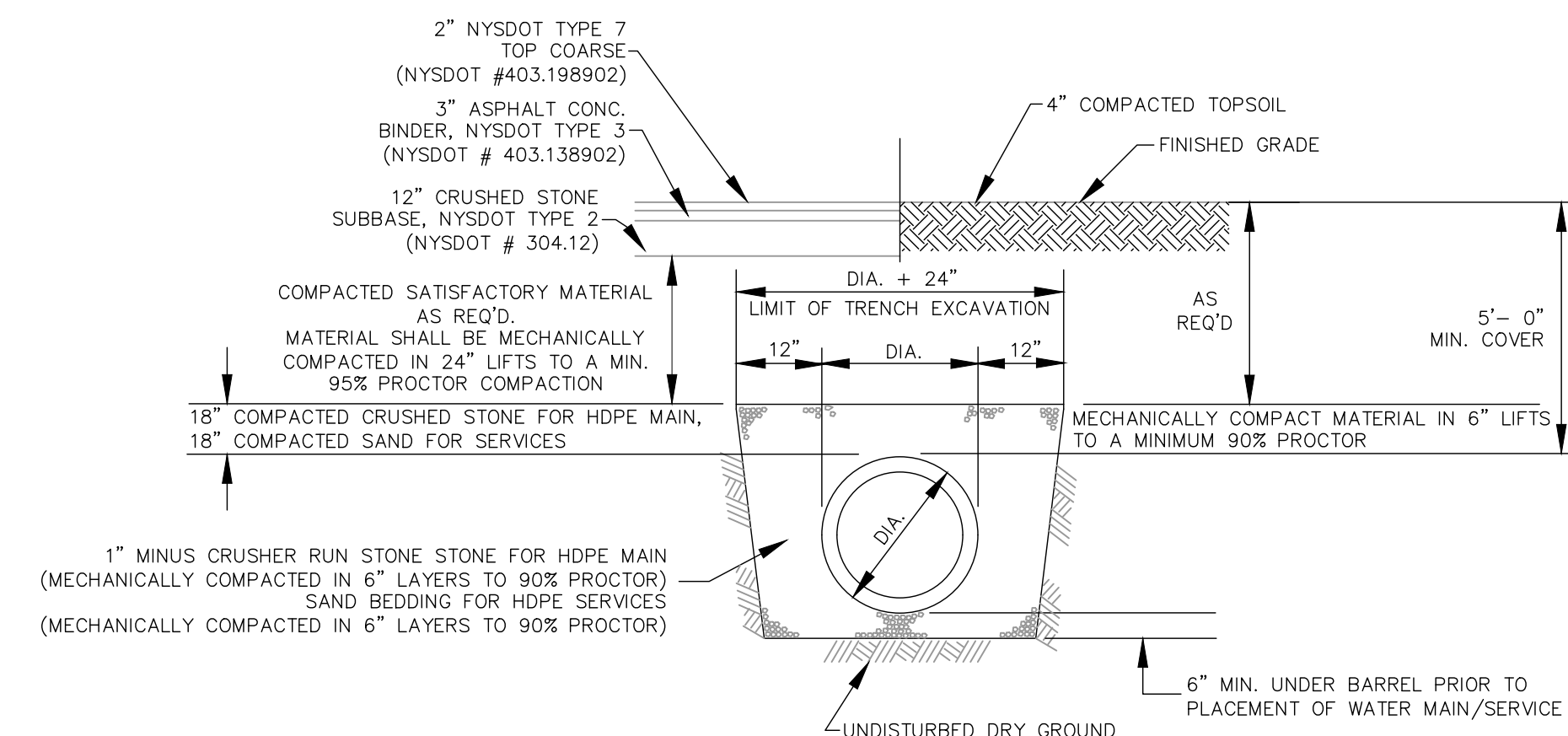
### TYPICAL DRIVEWAY CULVERT

NOT TO SCALE



### TYPICAL DRIVEWAY CROSSING

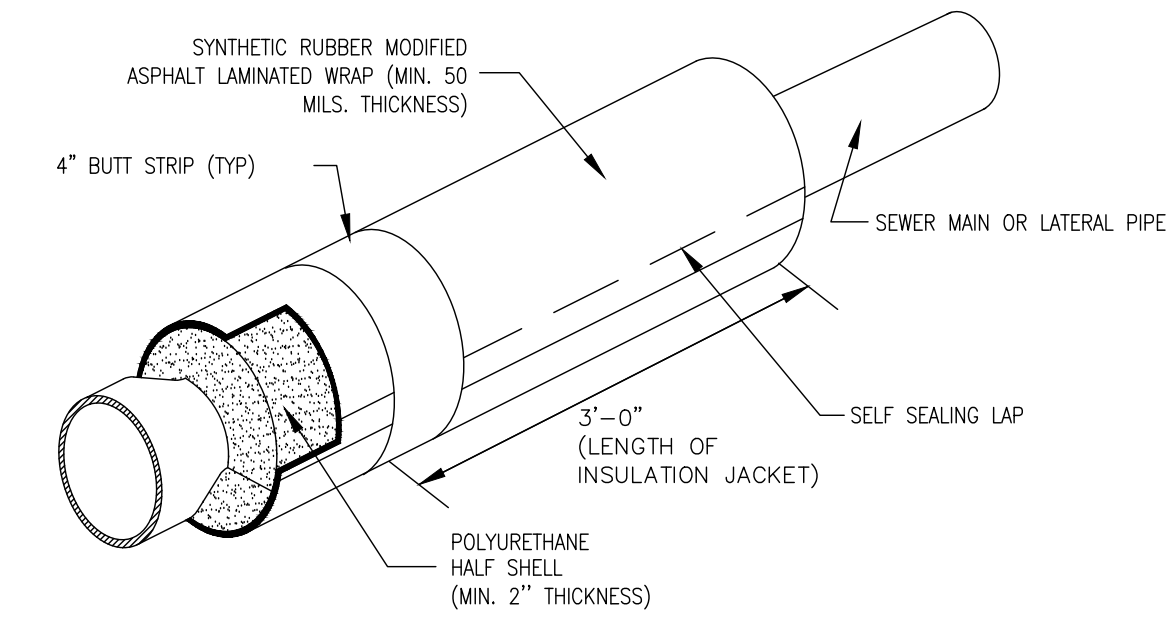
NOT TO SCALE



### TYPICAL SANITARY SEWER MAIN & SERVICE LINE DETAIL

N.T.S.

NOTE: ALL SANITARY SEWER MAIN AND SERVICE PIPING SHALL BE INSTALLED IN A DRY TRENCH.



NOTES:

- 1. A JACKET ACCELERANT IS REQUIRED FOR INSTALLATION TEMPERATURES UNDER 50°F IN ORDER FOR ADHESIVES TO SEAL.
2. THE 3" INSULATION SECTION SHALL BE INSTALLED SUCH THAT THE JOINTS BETWEEN EACH 3' SECTION ARE NOT GREATER THAN 1" OF EACH OTHER.

### TYPICAL SEWER MAIN & LATERAL INSULATING

N.T.S.

Table with columns: REVISIONS, REV DESCRIPTION, DATE. Includes record drawings date of August 2021.

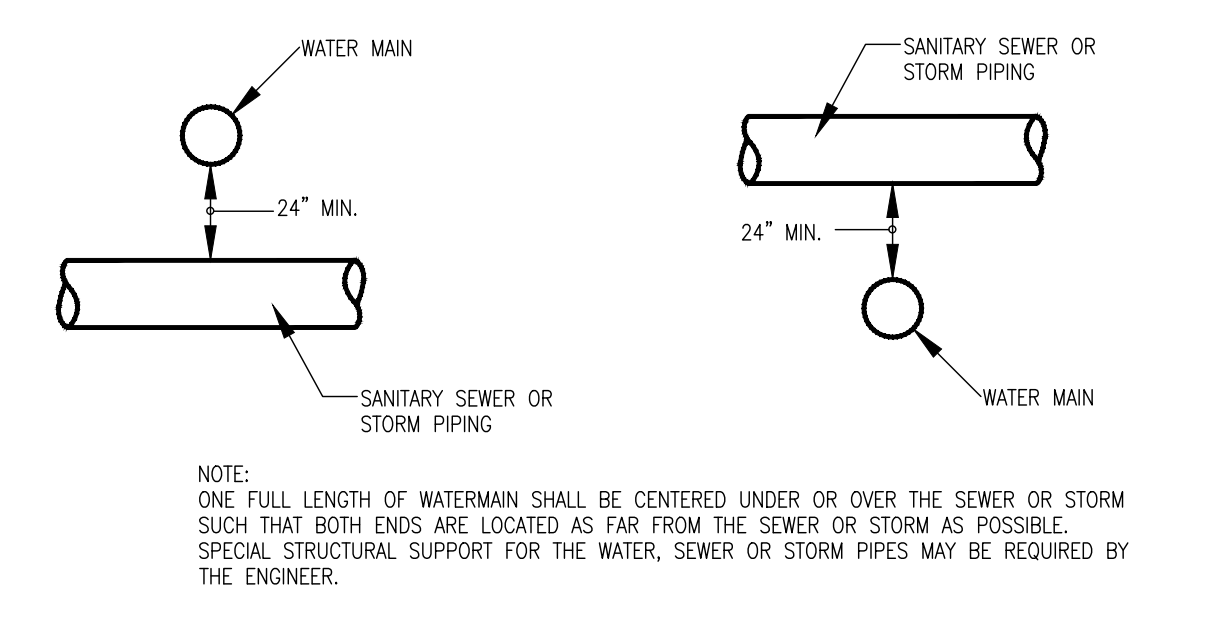


VERTICAL text on the left margin containing project and drawing information.

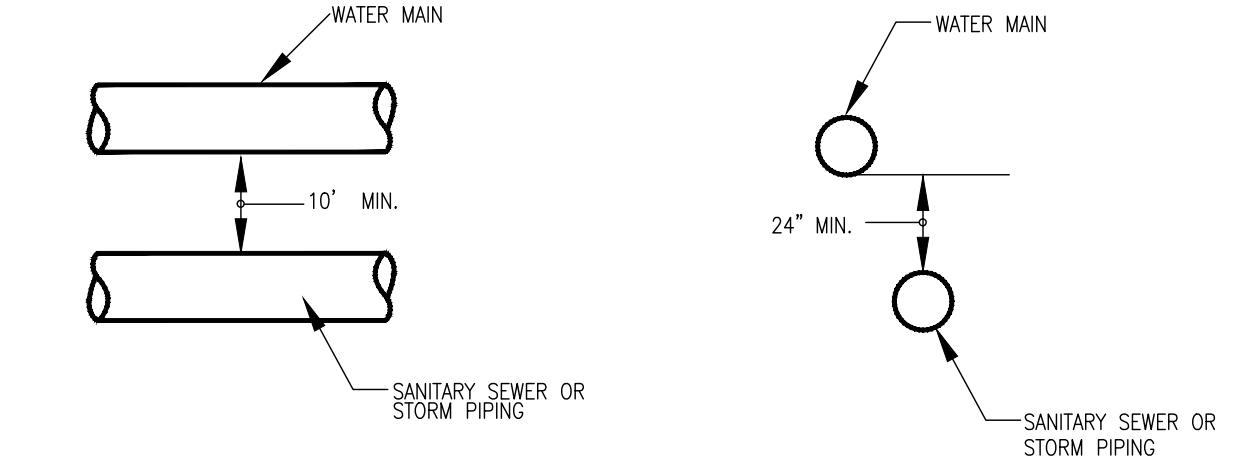


REVISIONS

REV	DESCRIPTION	DATE
FOR RECORD		AUGUST 2021



CROSSING PROVISIONS



HORIZONTAL AND VERTICAL SEPARATION PROVISIONS  
WATER/SEWER/STORM CROSSING  
AND SEPARATION DETAILS

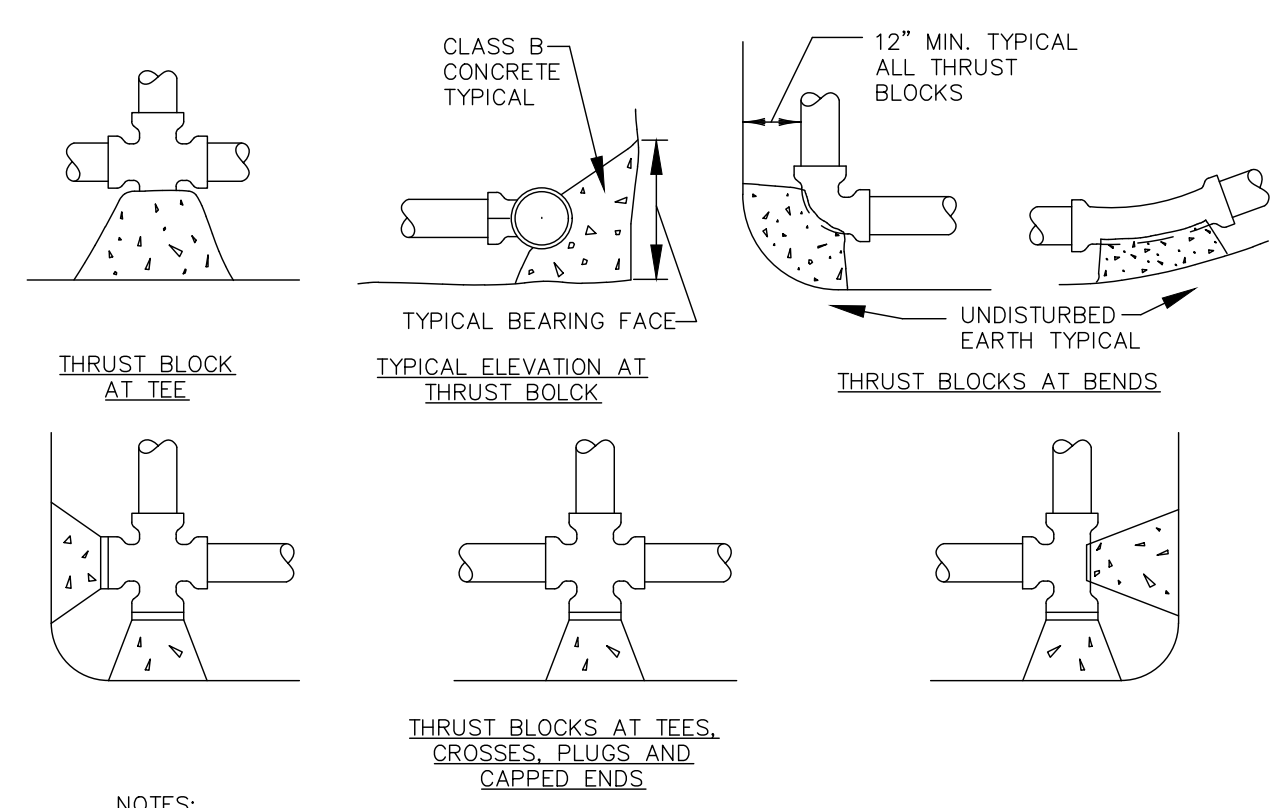
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C2AE

70 MAIN STREET, PO BOX 368, CANTON, NY 13617

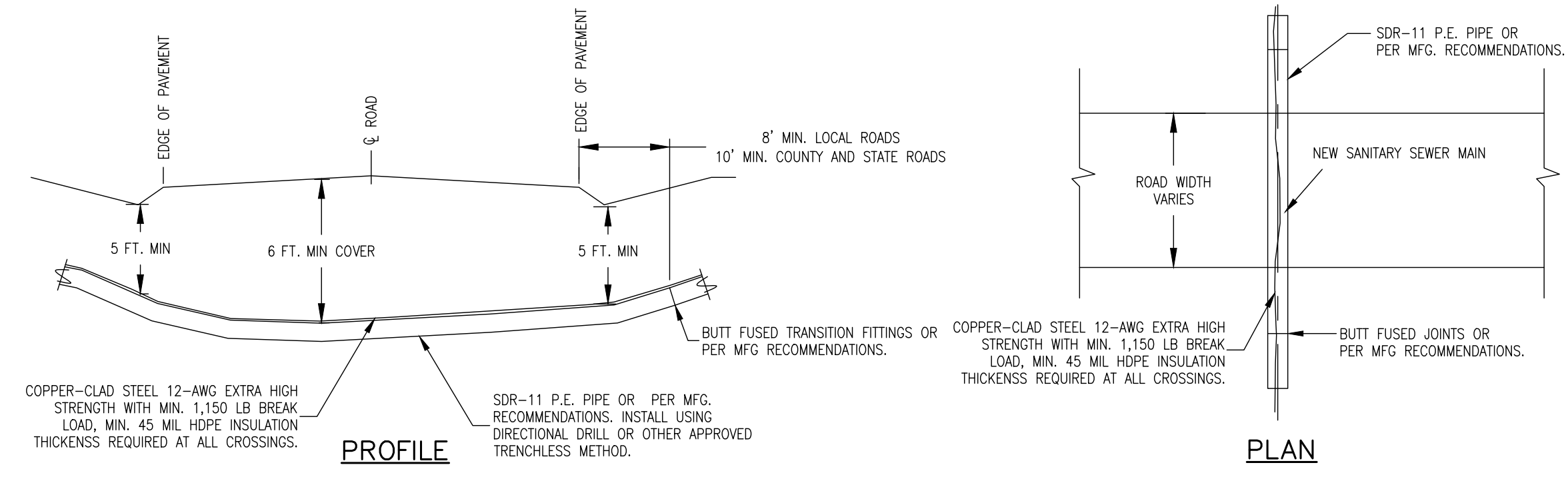
AREA OF BEARING FACE OF CONCRETE THRUST BLOCK IN SQ. FT. BLOCKS TO BE POURED AGAINST UNDISTURBED EARTH

PIPE SIZE	90° BEND	45° BEND	22.5° BEND	11.25° BEND	TEE OR PLUG
≤6"	6	4	3	3	9
8"	12	6	3	3	16
10"	18	12	6	3	24
12"	24	14	8	4	34
16"	44	24	12	6	62
20"	68	36	20	10	96

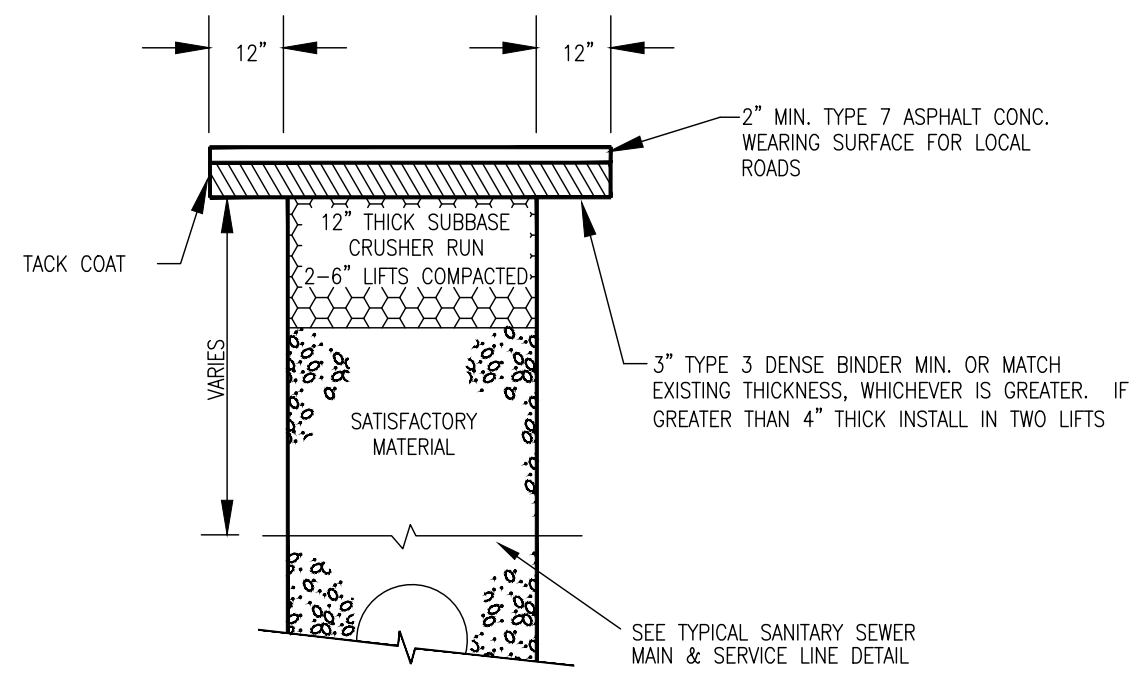
AREAS BASED ON AN INTERNAL PRESSURE OF 200 PSIG AND A SOIL BEARING PRESSURE OF 2000 PSF.



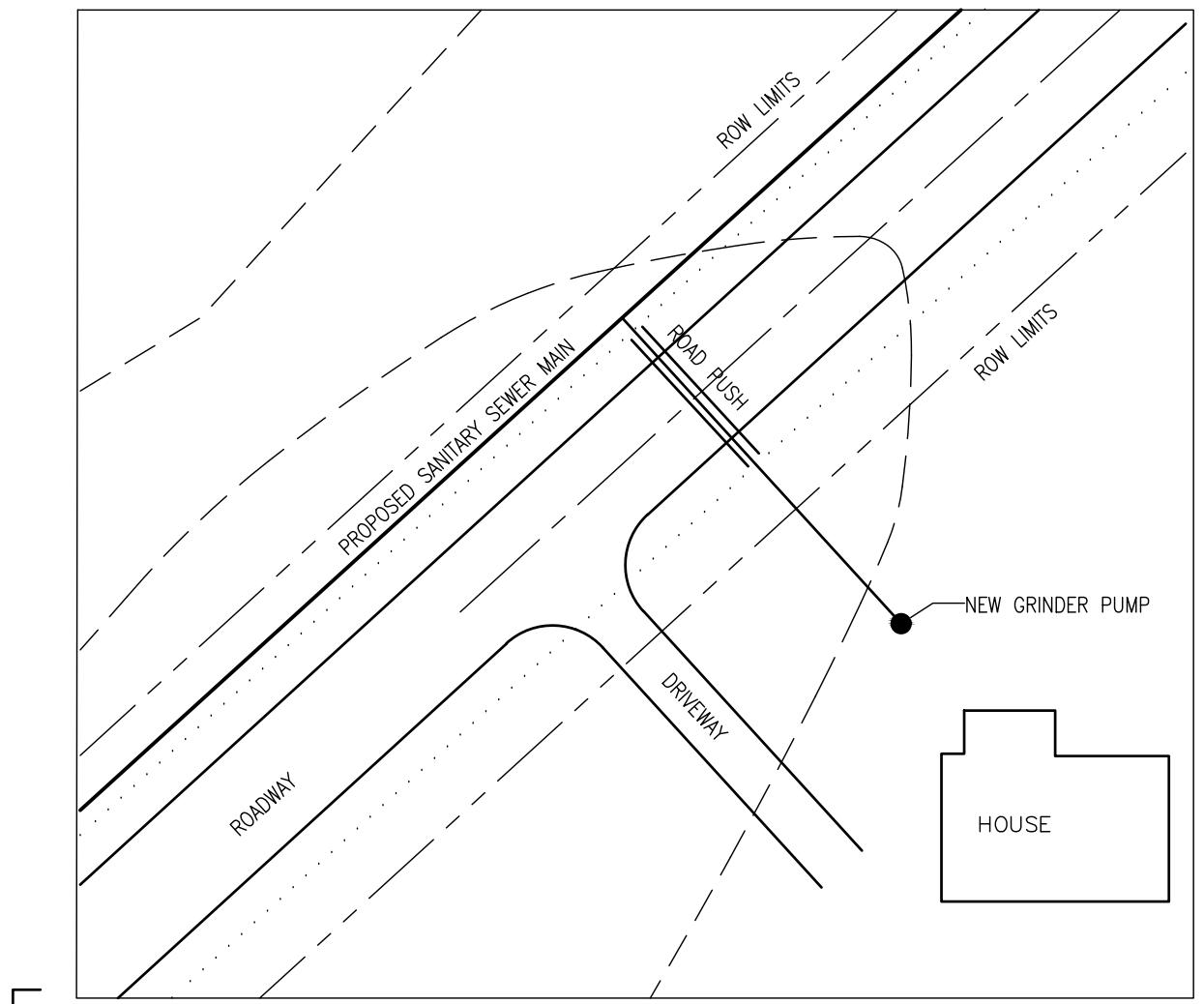
TYPICAL THRUST BLOCK  
N.T.S.



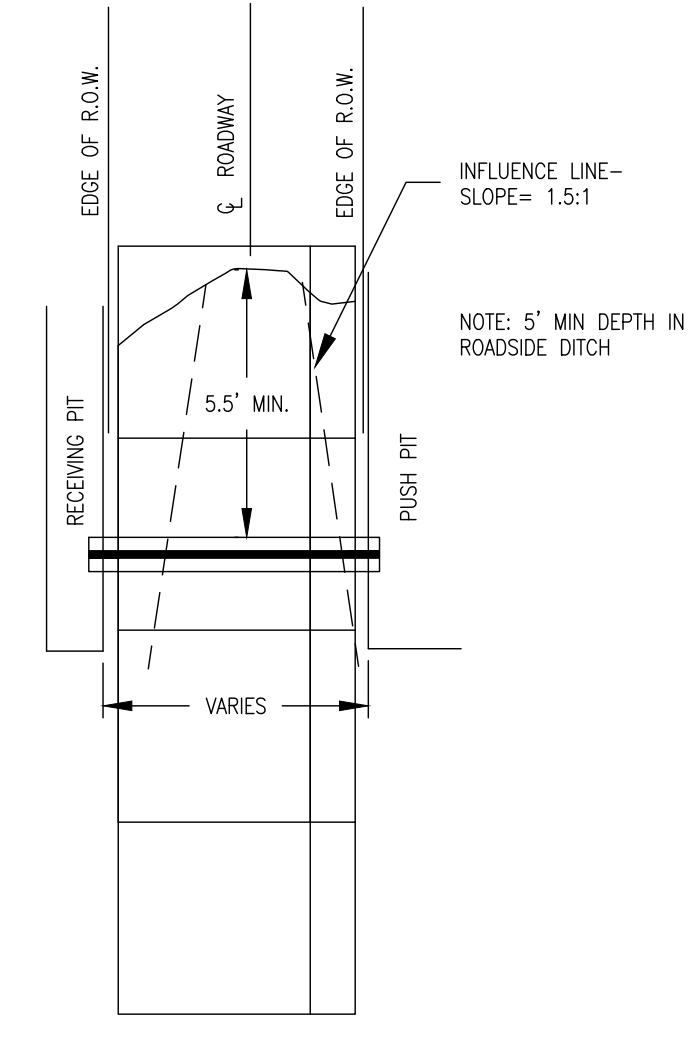
TYPICAL TRENCHLESS ROAD CROSSING  
N.T.S.



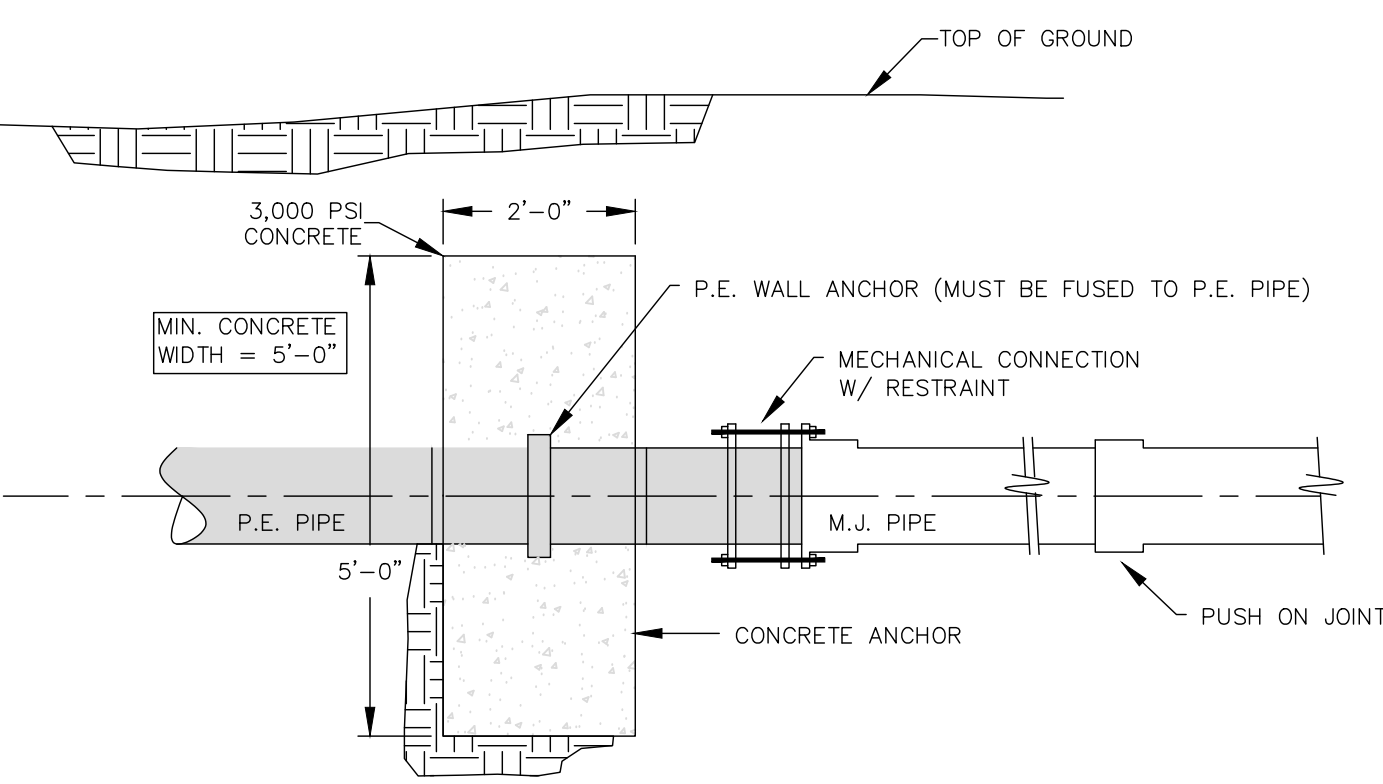
COUNTY & LOCAL PAVED  
TYPICAL ROAD PAVEMENT  
RESTORATION  
N.T.S.



TYPICAL SANITARY SEWER  
SERVICE LATERAL CROSSING  
SCALE: 1"=50' HORIZ.

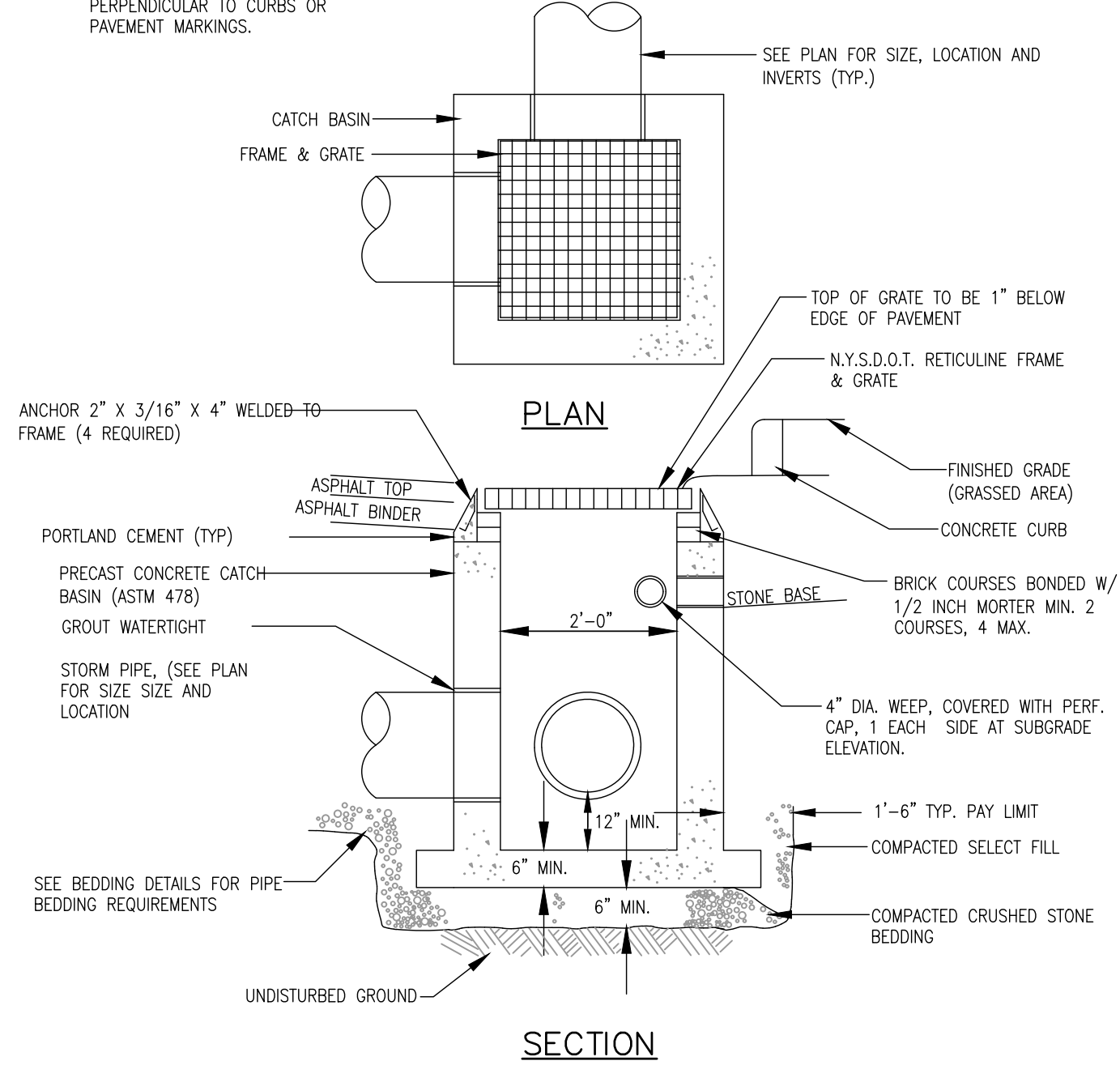


TYPICAL THRUST BLOCK  
SCALE: 1"=50' HORIZ.  
SCALE: 1"=5' VERT.

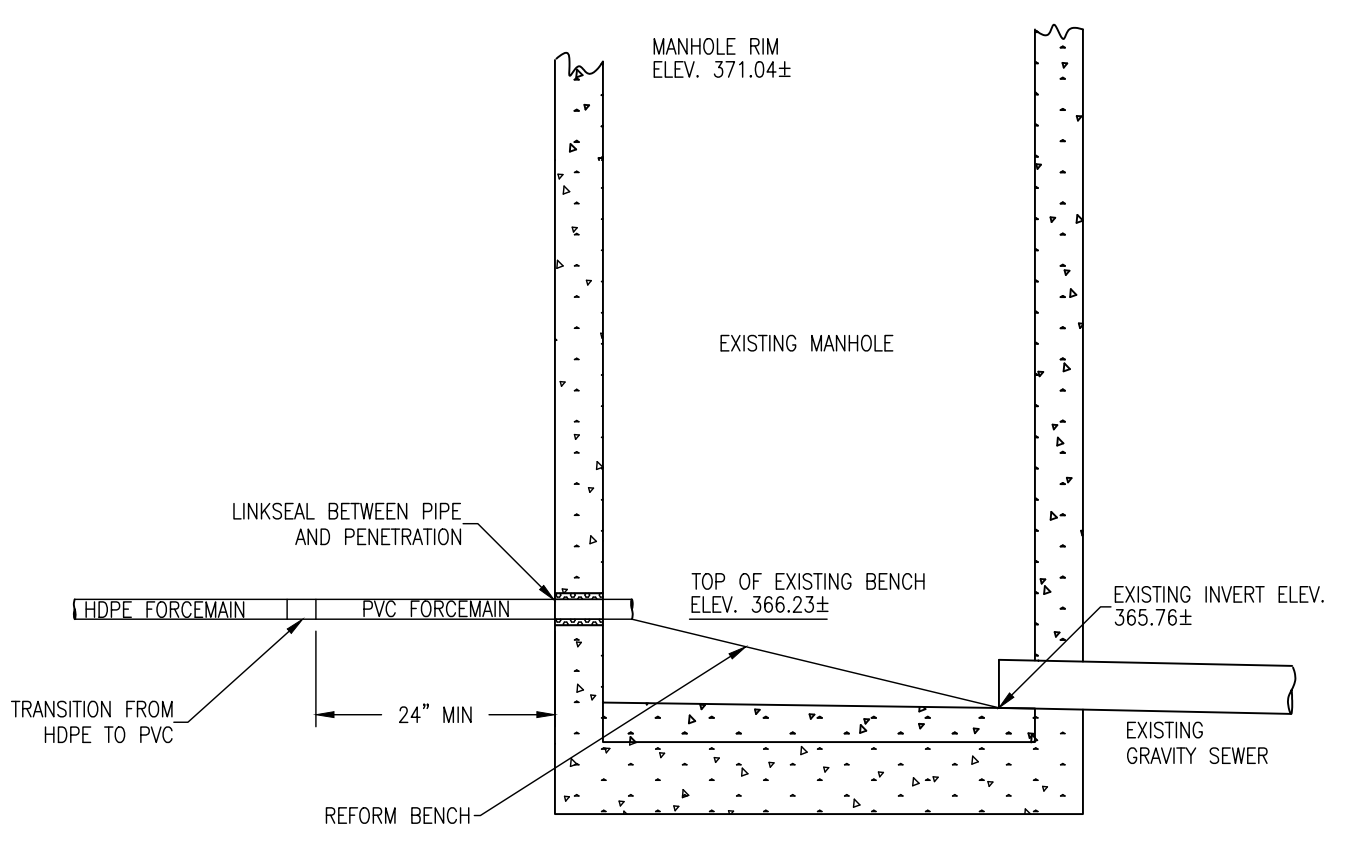


TYPICAL P.E. WALL ANCHOR  
N.T.S.

- NOTES:
- TEMPORARILY WRAP GRATE WITH FILTER FABRIC DURING CONSTRUCTION.
  - GRATES SET IN PAVED AREAS SHALL BE PLACED PARALLEL OR PERPENDICULAR TO CURBS OR PAVEMENT MARKINGS.



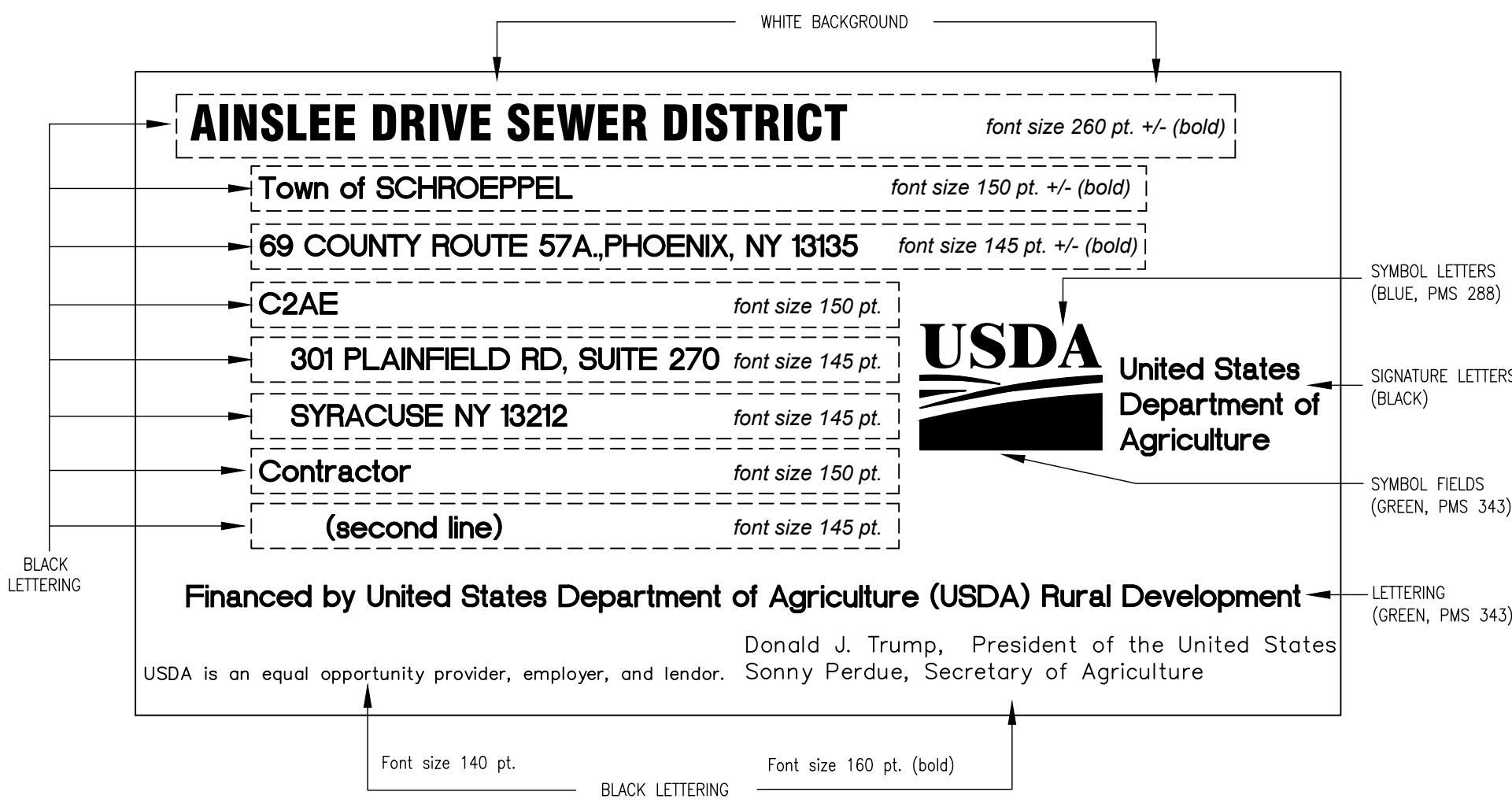
STORM INLET DETAIL  
N.T.S.



TYPICAL DROP CONNECTION LPSS TO EXISTING MANHOLE  
N.T.S.

PLOTTED BY: SANDRO, JACOB, 3/20/22 2:28 PM  
C:\071710003\_Sewer\RecordDrawings\RecordDrawings\Detail Sheets\Map - C-001.DETAILS  
C:\071710003\_Sewer\RecordDrawings\RecordDrawings\Detail Sheets\Map - C-001.DETAILS  
DESIGNED BY: SAU  
APPROVED BY: IFY  
CHECKED BY: DAB

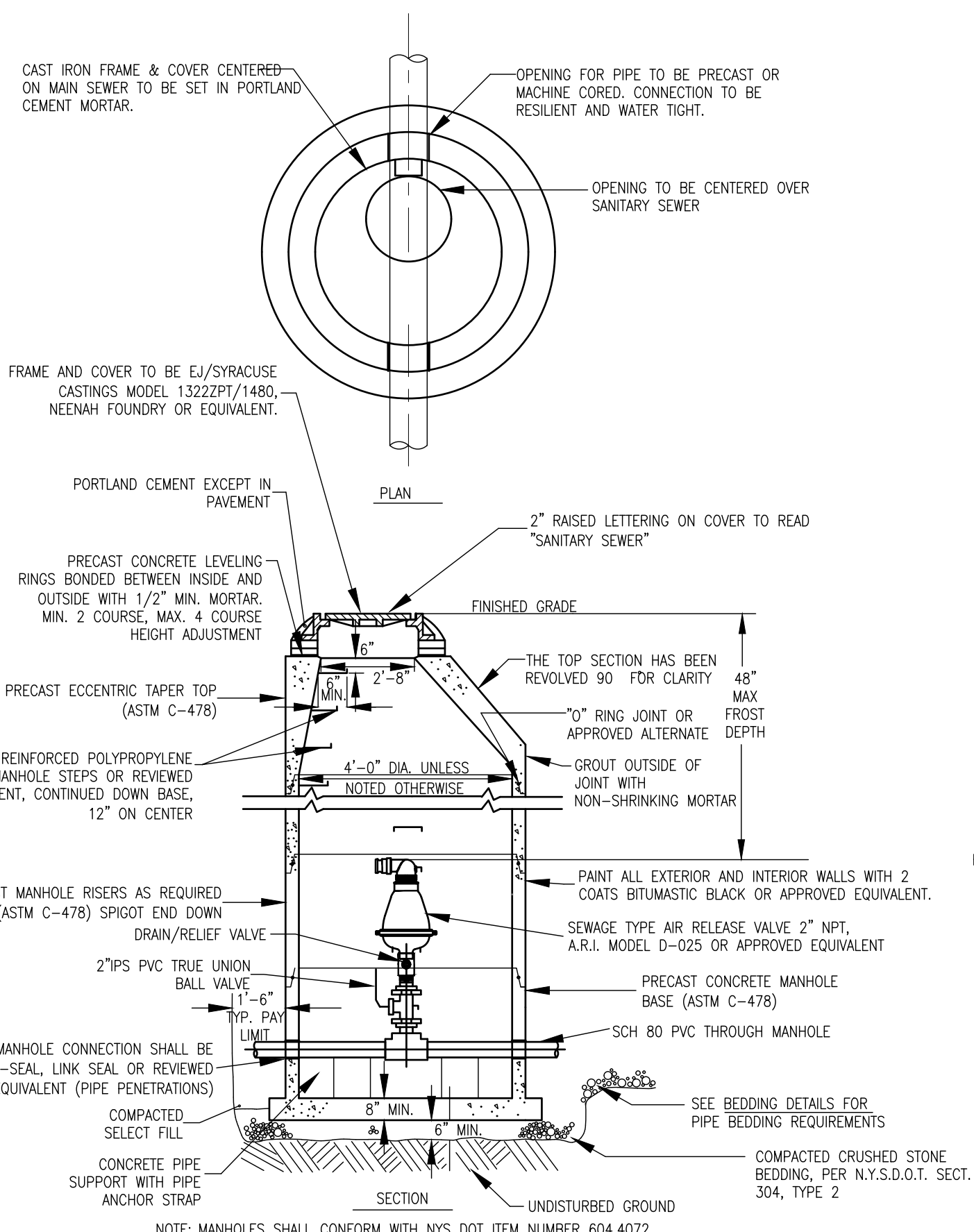
## TEMPORARY CONSTRUCTION SIGN FOR RURAL DEVELOPMENT PROJECTS



SIGN DIMENSIONS: 1200mm x 2400mm x 19mm (approx. 4' x 8' x 3/4")  
PLYWOOD PANEL (APA RATED A-B GRADE-EXTERIOR)

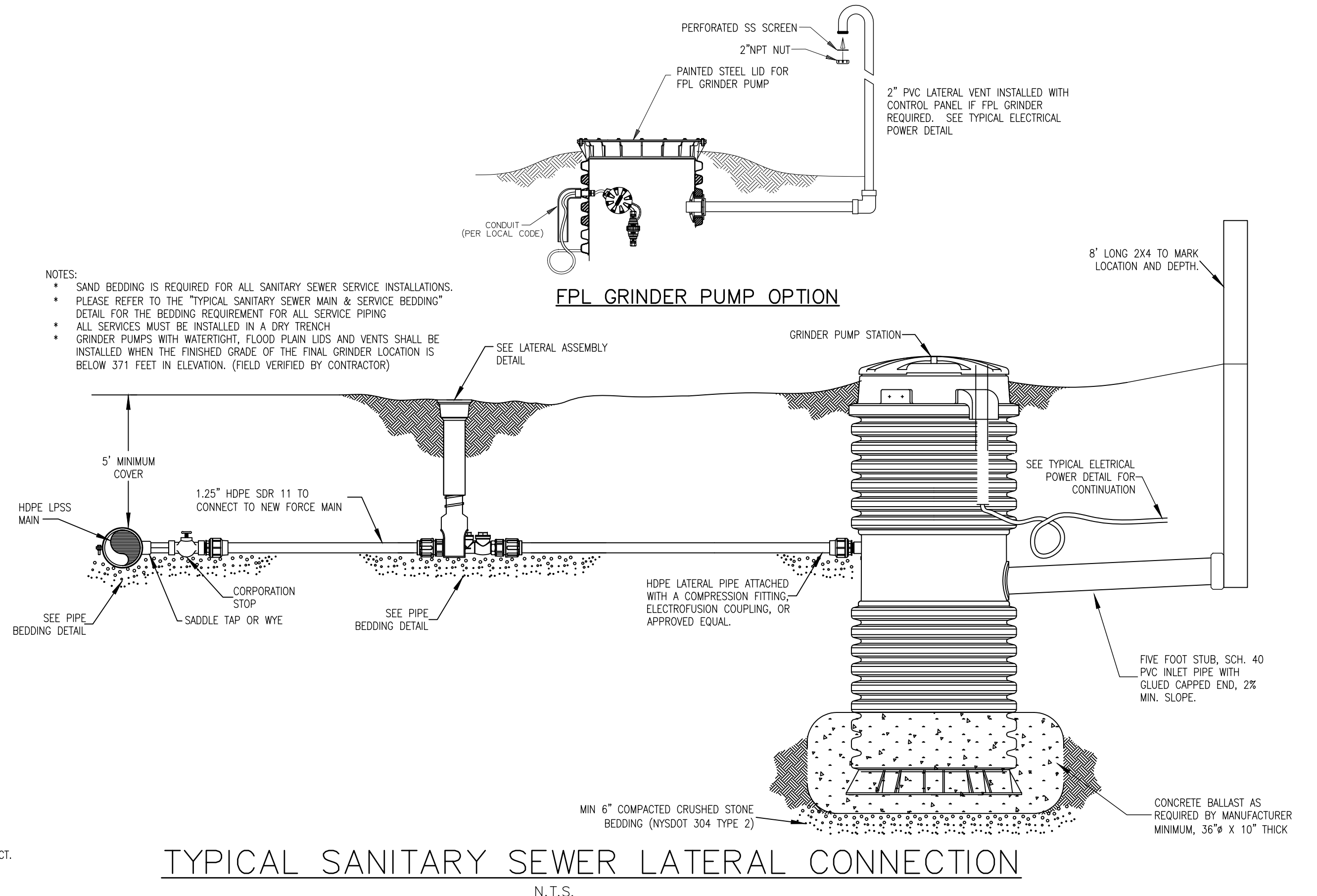
### TYPICAL CONSTRUCTION SIGN

N.T.S.



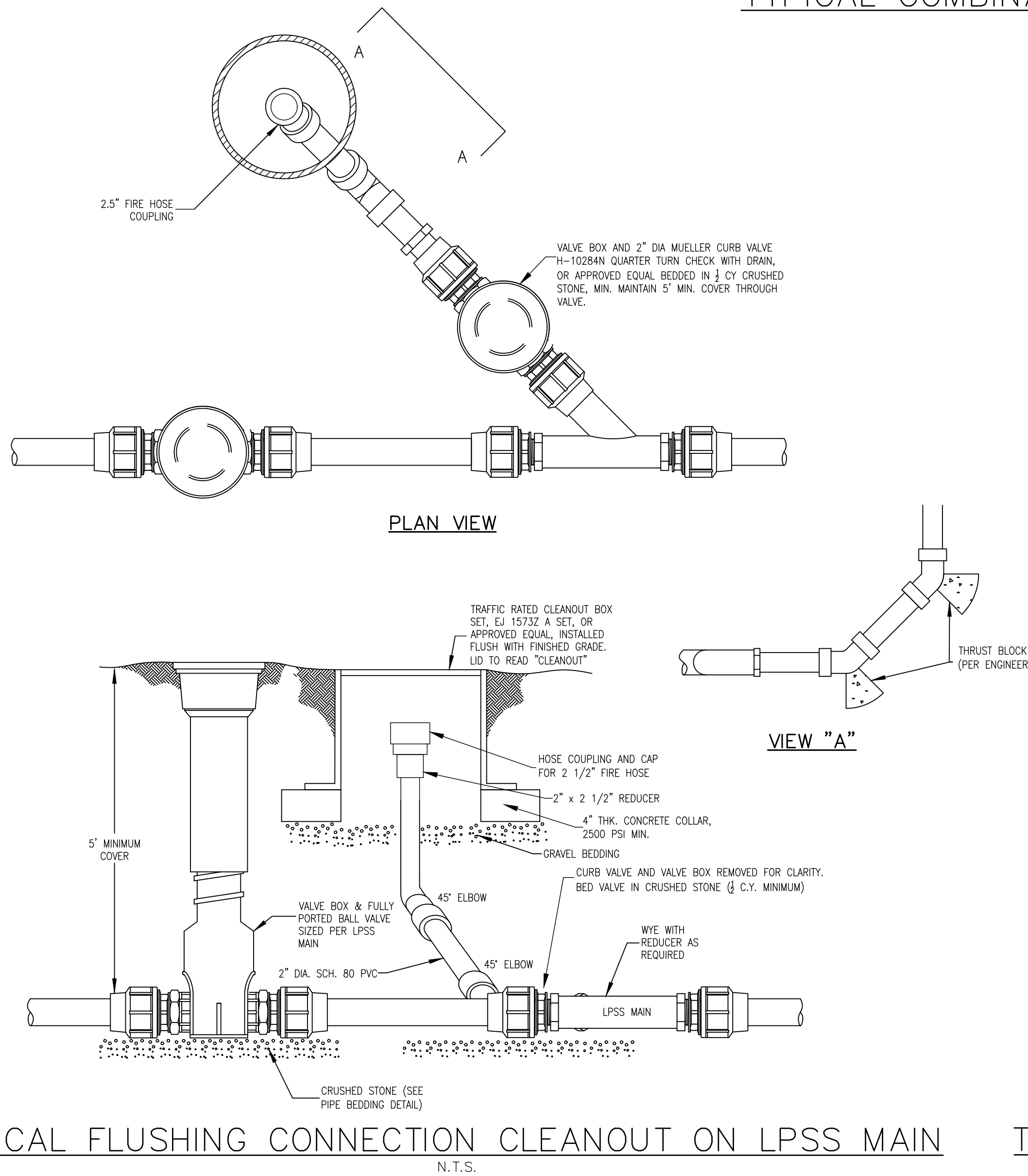
### TYPICAL COMBINATION AIR RELEASE VALVE STATION

N.T.S.



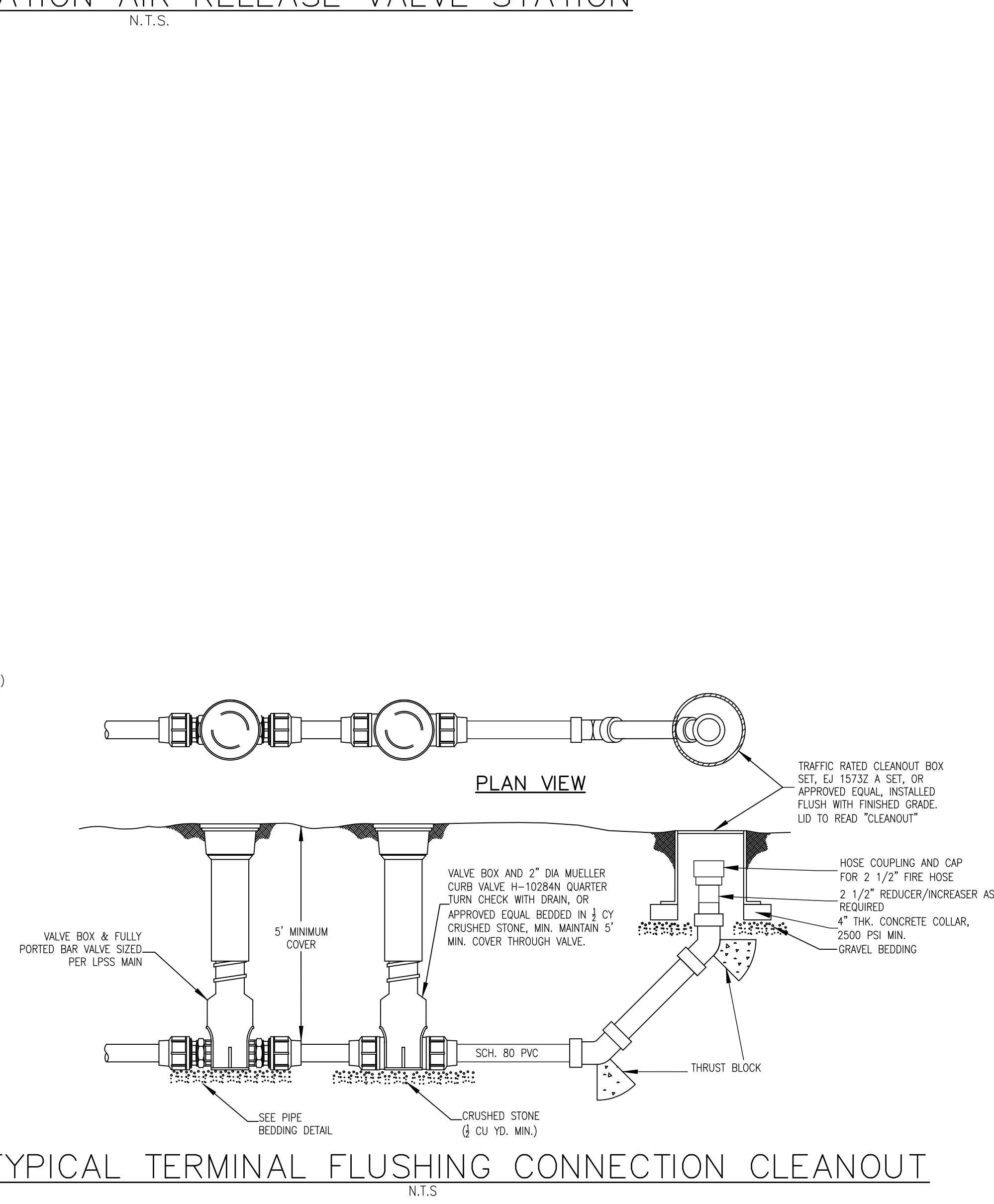
### TYPICAL SANITARY SEWER LATERAL CONNECTION

N.T.S.



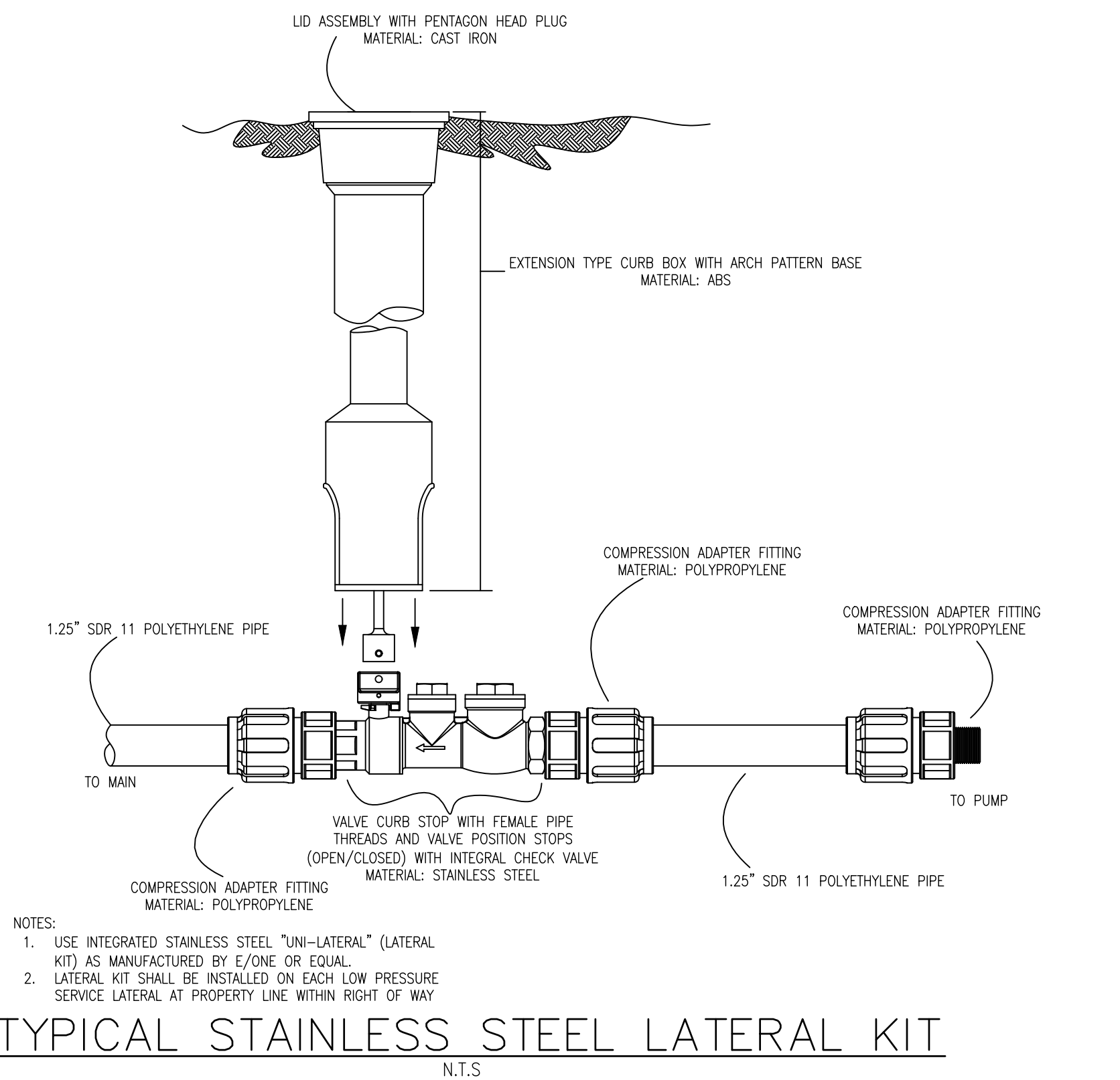
### TYPICAL FLUSHING CONNECTION CLEANOUT ON LPSS MAIN

N.T.S.



### TYPICAL TERMINAL FLUSHING CONNECTION CLEANOUT

N.T.S.

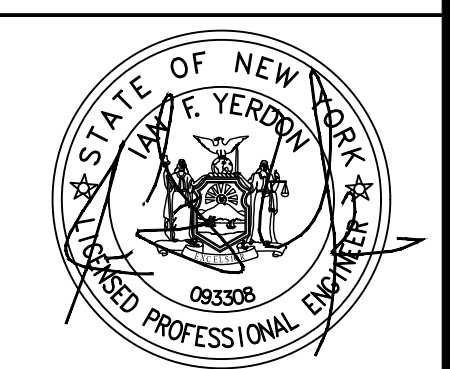


### TYPICAL STAINLESS STEEL LATERAL KIT

N.T.S.

- NOTES:  
1. USE INTEGRATED STAINLESS STEEL "UNI-LATERAL" (LATERAL KIT) AS MANUFACTURED BY E/ONE OR EQUAL.  
2. LATERAL KIT SHALL BE INSTALLED ON EACH LOW PRESSURE SERVICE LATERAL AT PROPERTY LINE WITHIN RIGHT OF WAY

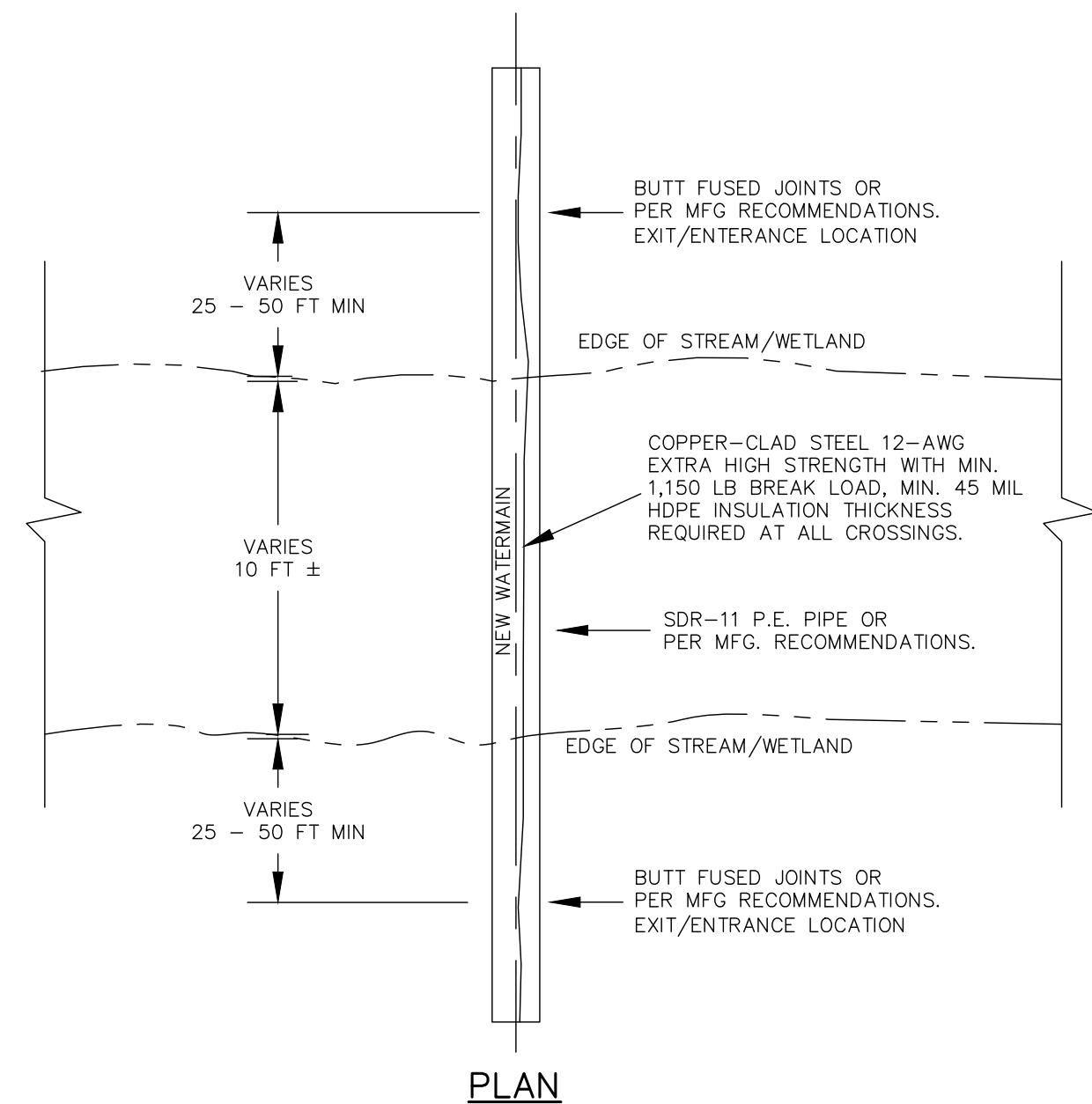
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C2AE



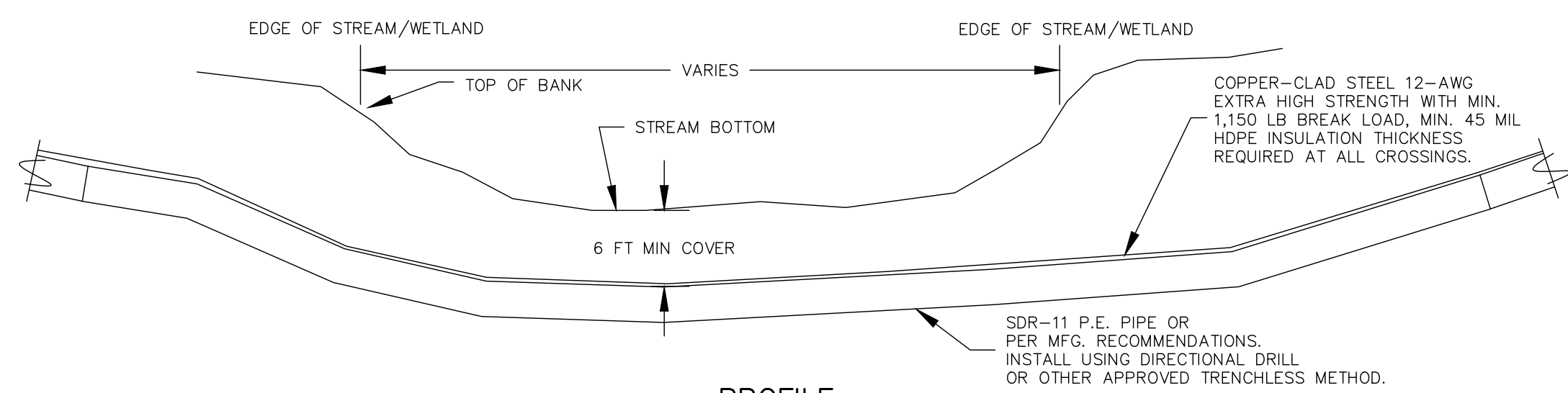
REVISIONS	REV DESCRIPTION DATE
FOR RECORD	AUGUST 2021



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PLAN



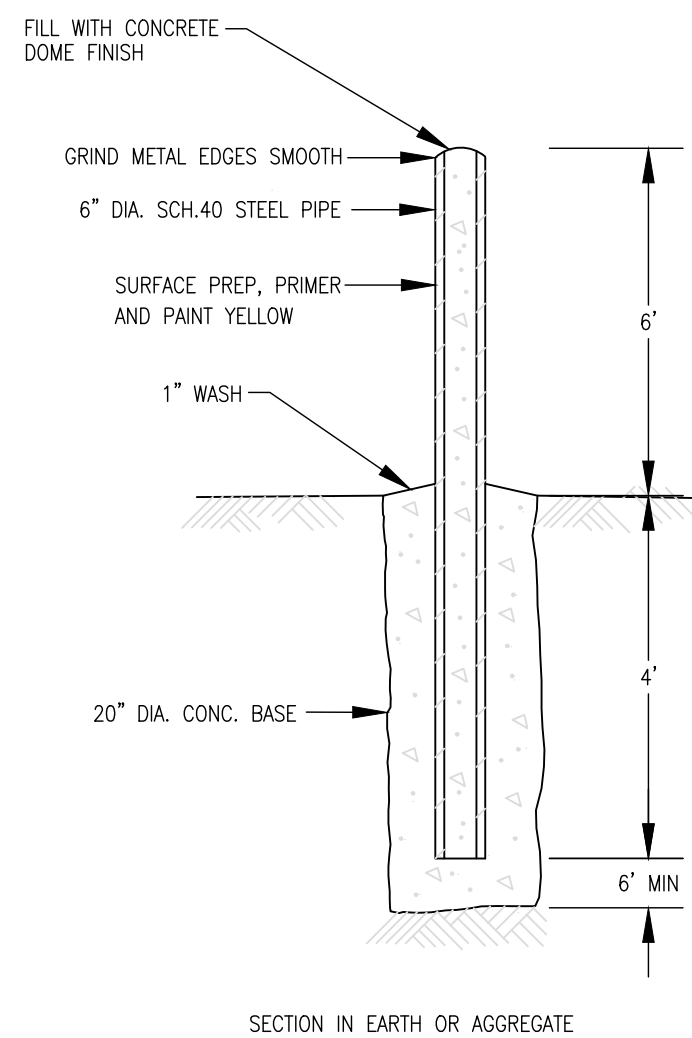
PROFILE

NOTE:

- A MINIMUM OF 5 FULL PIPE LENGTHS (FULL LENGTH BEING 18' OR 20') SHALL BE RESTRAINED WITH LOCKING RESTRAINT GASKETS OR BELL CLAMP RESTRAINTS WHEN CONNECTING TO HDPE WATER MAIN. THIS IS REQUIRED EVEN IF CONCRETE PE WALL ANCHORS ARE ALSO INSTALLED. REFER TO SHEET C-003 GENERAL NOTE #10 FOR MORE DETAILS.
- ANY AND ALL SILT FENCE OR EROSION CONTROL MEASURES SHALL BE INSTALLED PRIOR TO ANY TRENCHLESS INSTALLATION ACTIVITIES OR PRIOR TO ANY WATER MAIN INSTALLATION.

TYPICAL TRENCHLESS STREAM/WETLAND CROSSING

N.T.S.

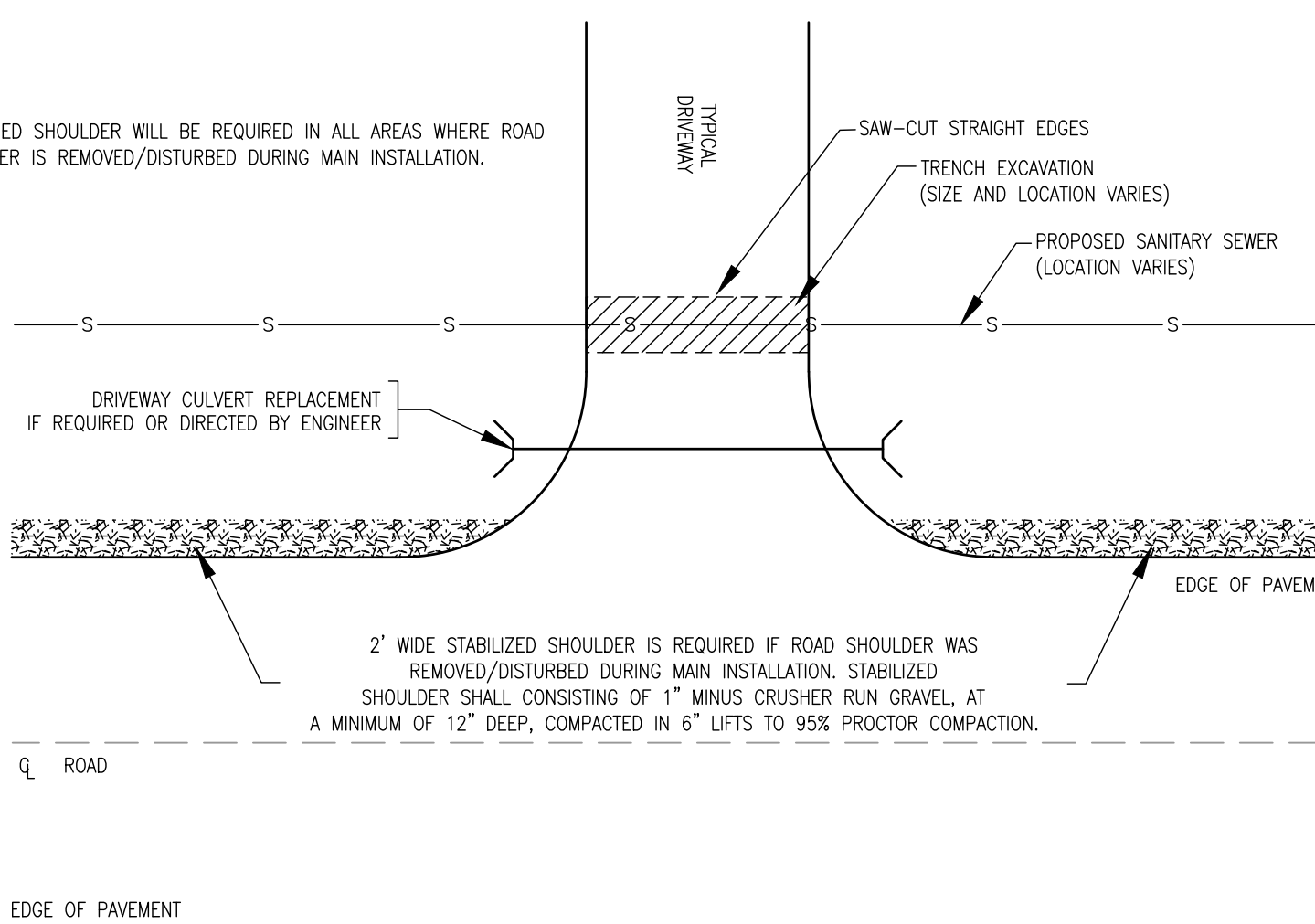


SECTION IN EARTH OR AGGREGATE

TYPICAL BOLLARD

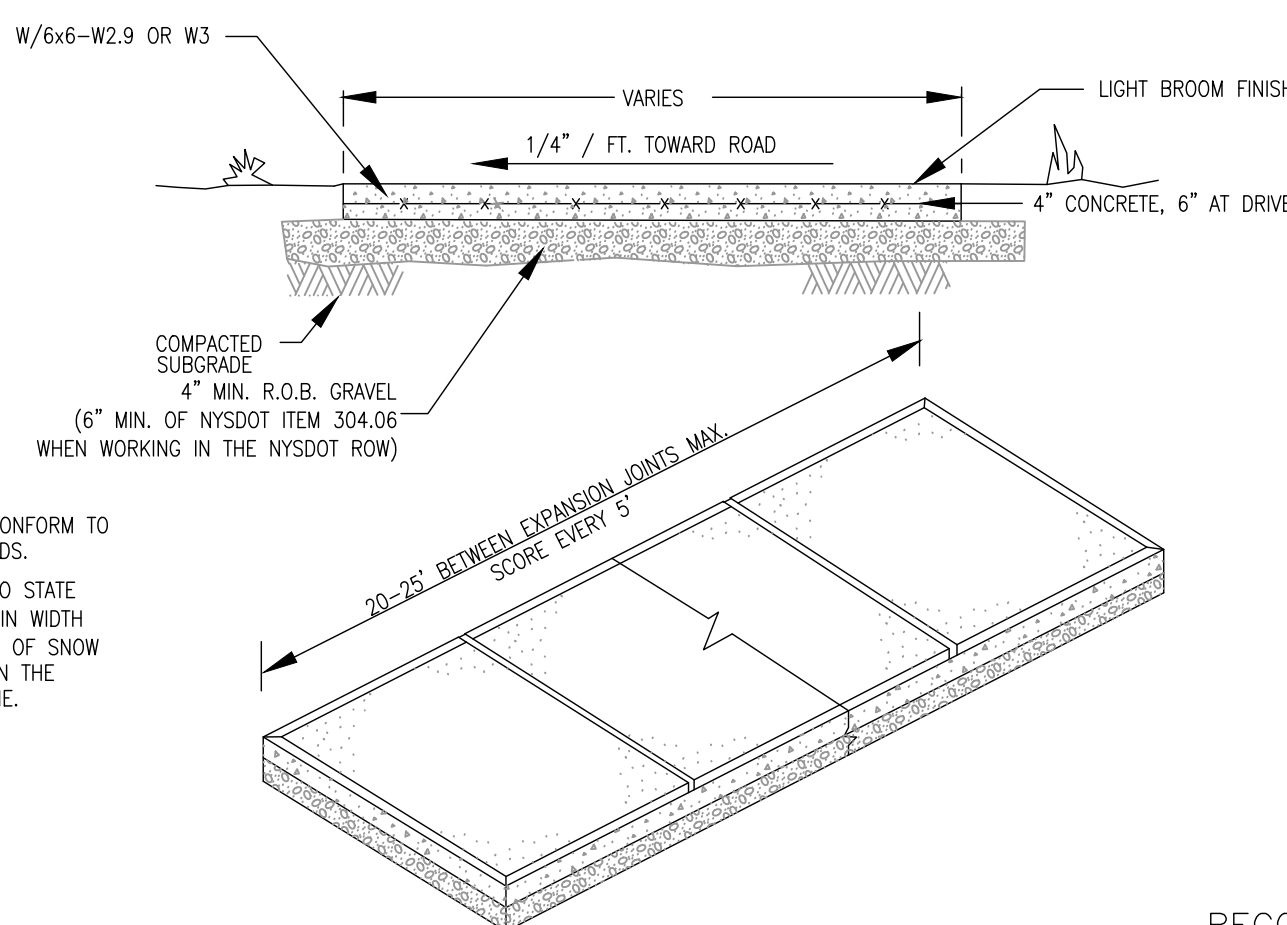
N.T.S.

NOTE: STABILIZED SHOULDER WILL BE REQUIRED IN ALL AREAS WHERE ROAD SHOULDER IS REMOVED/DISTURBED DURING MAIN INSTALLATION.



TYPICAL ROAD SHOULDER RESTORATION

N.T.S.



TYPICAL SIDEWALK

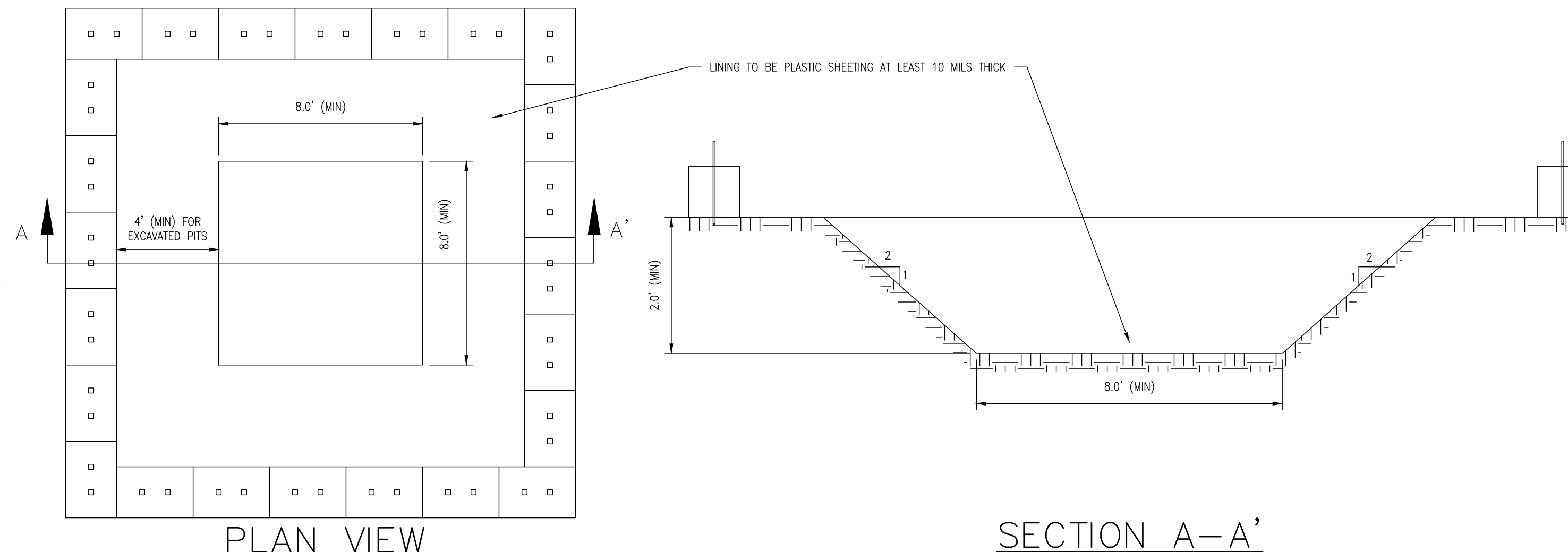
N.T.S.

NOTE: ALL SIDEWALKS SHALL CONFORM TO CURRENT ADA STANDARDS. SIDEWALKS ADJACENT TO STATE ROADS SHALL BE 5.0' IN WIDTH WHILE MAINTAINING 5.0' OF SNOW STORAGE AREA BETWEEN THE SIDEWALK AND CURBLINE.

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70 MAIN STREET, PO BOX 368, CANTON, NY 13617



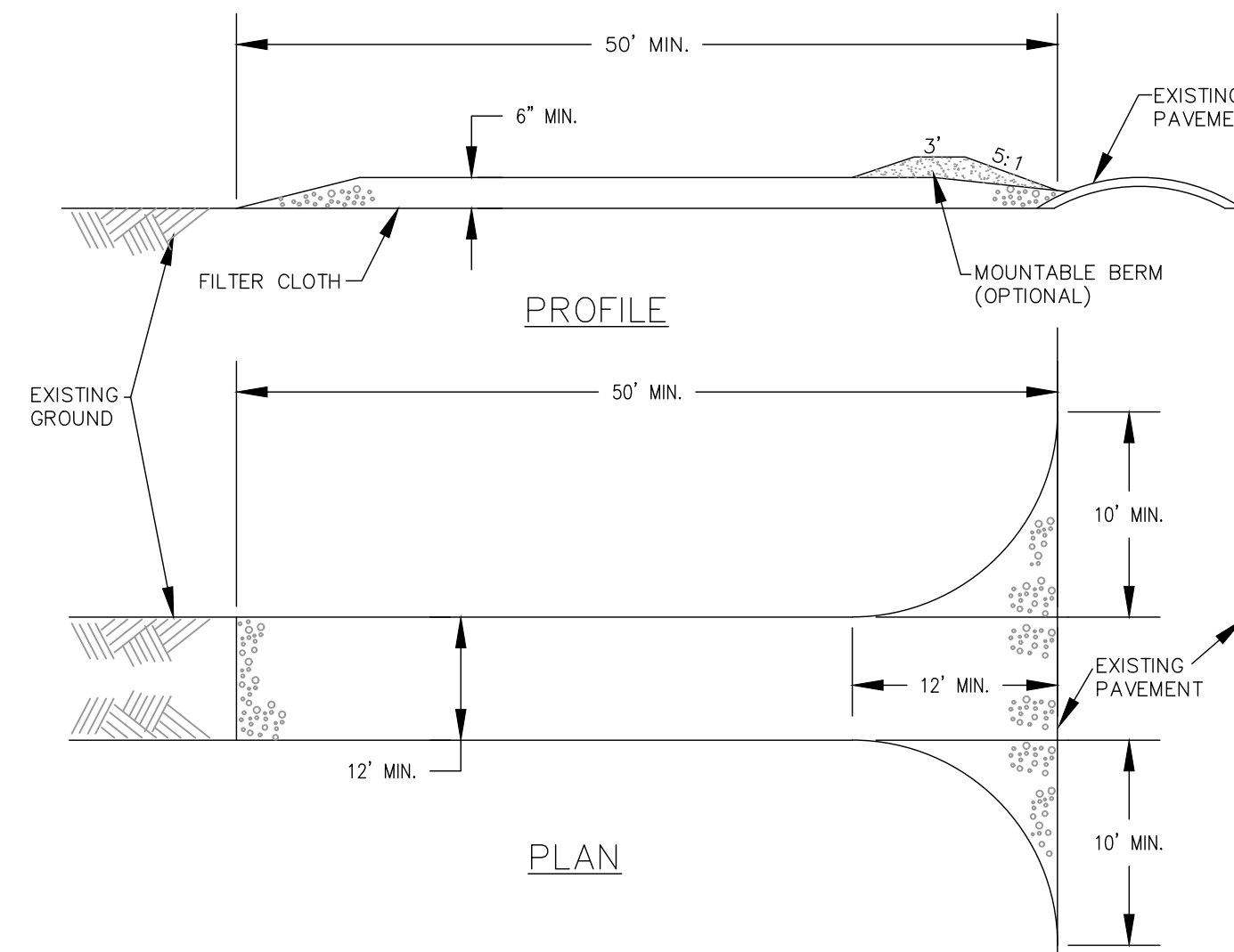
PLAN VIEW

SECTION A-A'

TYPICAL CONCRETE WASHOUT PIT

N.T.S.

- CAPACITY TO BE SIZED TO THE MINIMUM DIMENSIONS ABOVE OR TO CONTAIN SOLIDS, WASH WATER, AND RAIN WATER TRIBUTARY TO THE PIT, WHICHEVER IS LARGER. WASH WATER TO BE ESTIMATED AS 7 GALLONS PER CHUTE AND 30 GALLONS PER HOPPER OF THE CONCRETE TRUCK.
- ACCUMULATED HARDENED MATERIAL IS TO BE REMOVED BEFORE THE WASHOUT PIT REACHES 75% OF STORAGE CAPACITY AND DISPOSED OF IN AN OFFSITE LANDFILL. WASH WATER IS TO BE BROUGHT OFF SITE IN PROPER CONTAINMENT VESSEL.
- DISPOSAL OF HARDENED MATERIAL IS NOT PERMITTED ON SITE.
- REPLACE PLASTIC LINER AFTER EACH CLEANING OF THE WASHOUT PIT.
- CONSTRUCTION MUST BE IN ACCORDANCE WITH THE LATEST VERSION OF THE NEW YORK STATE GUIDELINES FOR URBAN EROSION & SEDIMENT CONTROL, STANDARD SPECIFICATIONS FOR CONCRETE WASHOUT PIT.



CONSTRUCTION SPECIFICATIONS

- STONE SIZE- USE 2" STONE, OR RECLAIMED OR RECYCLED CONCRETE EQUIVALENT.
- LENGTH- NOT LESS THAN 50 FEET (EXCEPT ON A SINGLE RESIDENCE LOT WHERE A 30 FOOT MINIMUM LENGTH WOULD APPLY.)
- THICKNESS- NOT LESS THAN SIX (6) INCHES.
- WIDTH- TWELVE (12) FOOT MINIMUM, BUT NOT LESS THAN FULL WIDTH AT POINTS WHERE INGRESS OR EGRESS OCCURS. TWENTY-FOUR (24) FOOT IF SINGLE ENTRANCE TO SITE.
- FILTER CLOTH- WILL BE PLACED OVER ENTIRE AREA PRIOR TO PLACING OF STONE.
- SURFACE WATER- ALL SURFACE WATER FLOWING OR DIVERTED TOWARD CON- STRUCTION ENTRANCES SHALL BE PIPED ACROSS THE ENTRANCE. IF PIPING IS IMPRACTICAL, A MOUNTABLE BERM WITH 5:1 SLOPES WILL BE PERMITTED.
- MAINTENANCE- THE ENTRANCE SHALL BE MAINTAINED IN A CONDITION WHICH WILL PREVENT TRACKING OR FLOWING OF SEDIMENT ONTO PUBLIC RIGHTS-OF-WAY. ALL SEDIMENT SPILLED, DROPPED, WASHED OF TRACTED ONTO PUBLIC RIGHTS-OF-WAY MUST BE REMOVED IMMEDIATELY.
- WHEN WASHING IS REQUIRED, IT SHALL BE DONE ON AN AREA STABILIZED WITH STONE AND WHICH DRAINS INTO AN APPROVED SEDIMENT TRAPPING DEVICE.
- PERIODIC INSPECTION AND NEEDED MAINTENANCE SHALL BE PROVIDED AFTER EACH RAIN.
- CONSTRUCTION MUST BE IN ACCORDANCE WITH THE LATEST VERSION OF THE NEW YORK STATE GUIDELINES FOR URBAN EROSION & SEDIMENT CONTROL, STANDARD SPECIFICATIONS FOR STABILIZED CONSTRUCTION ENTRANCE.

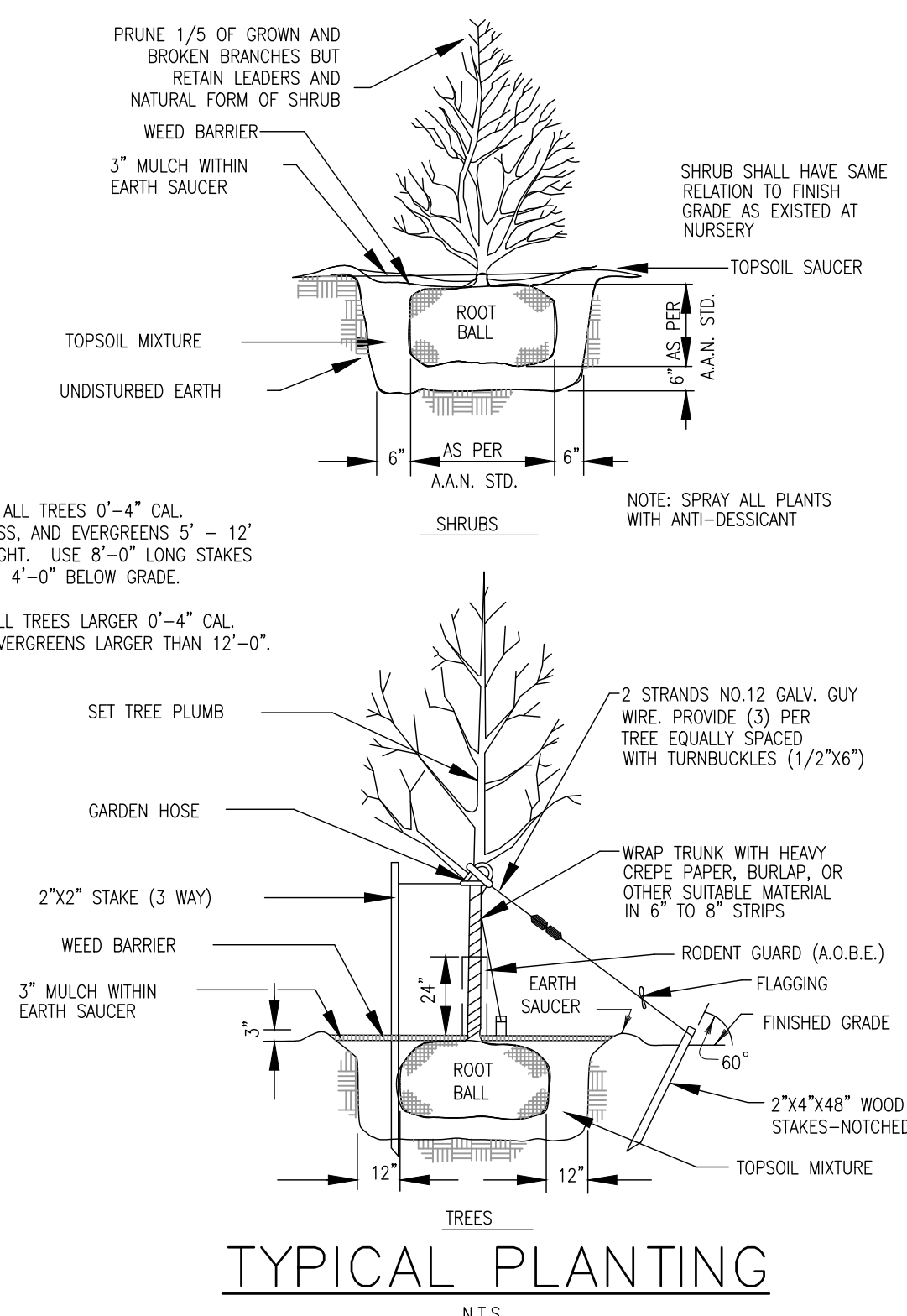
STABILIZED CONSTRUCTION ENTRANCE DETAIL

N.T.S.

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TYPICAL PLANTING

N.T.S.

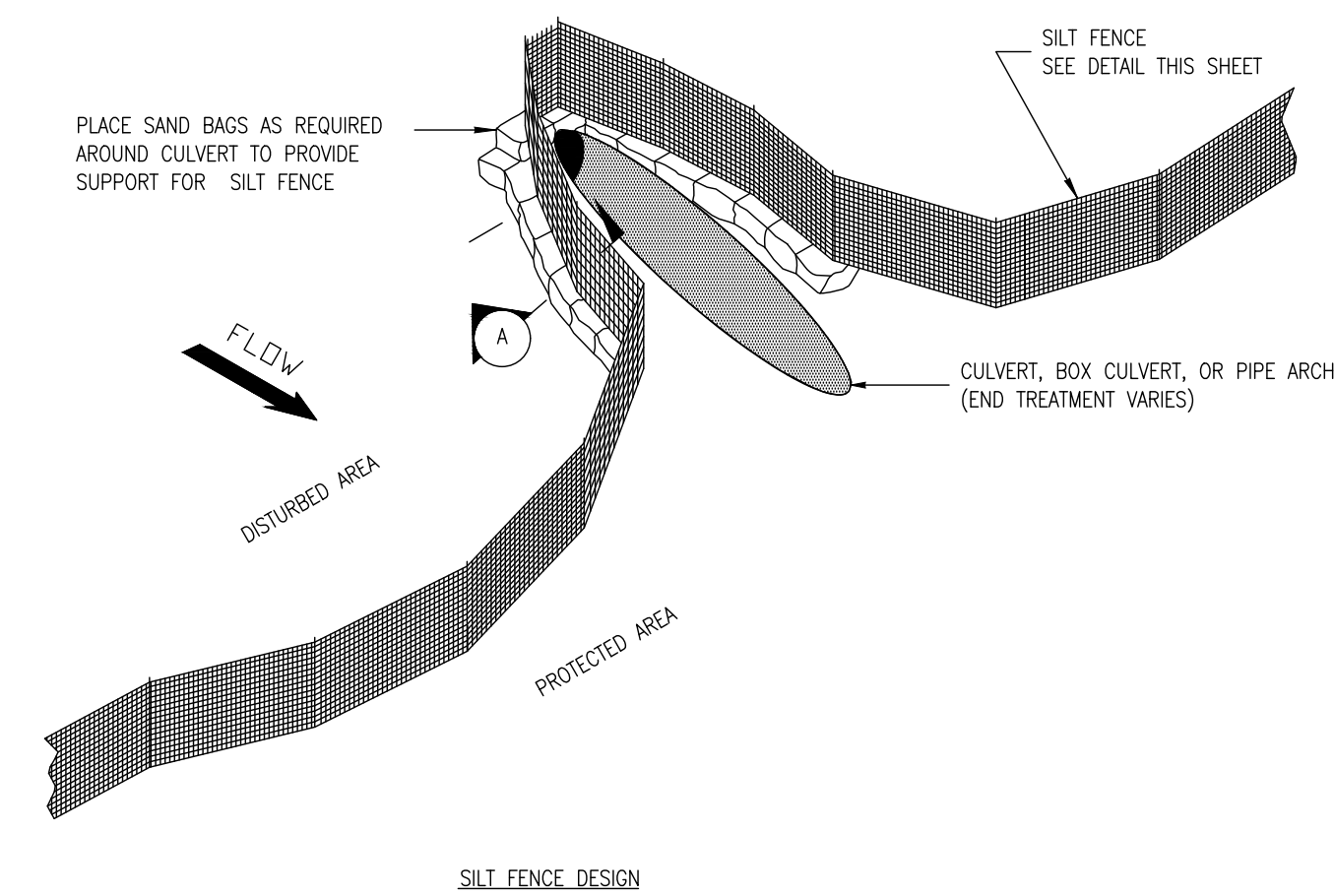
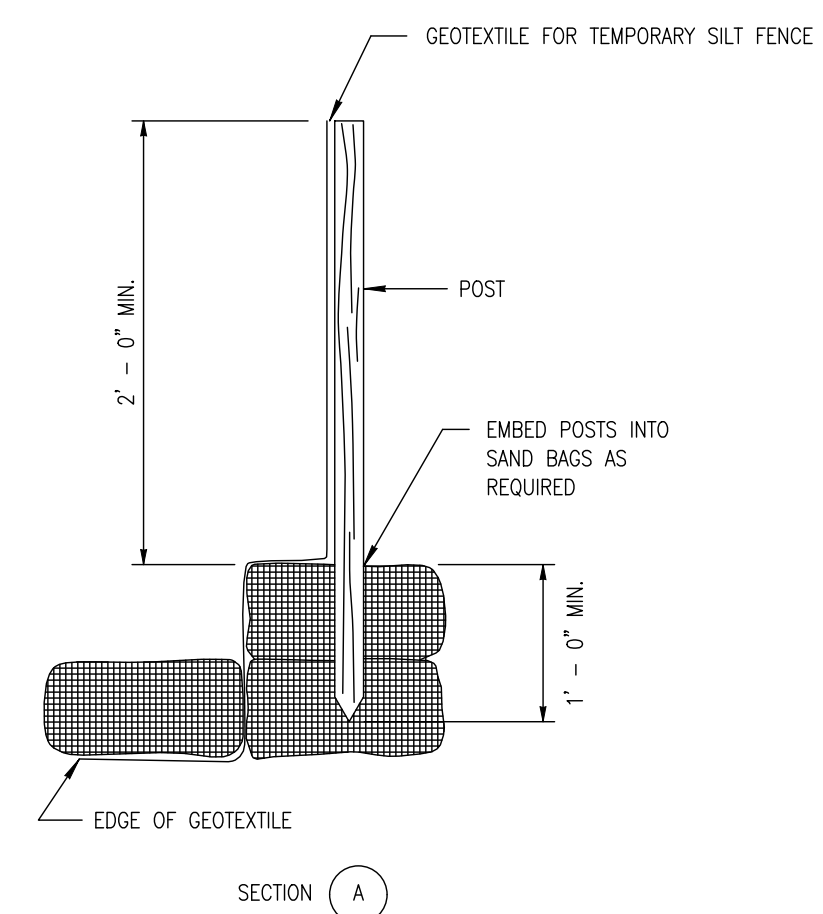
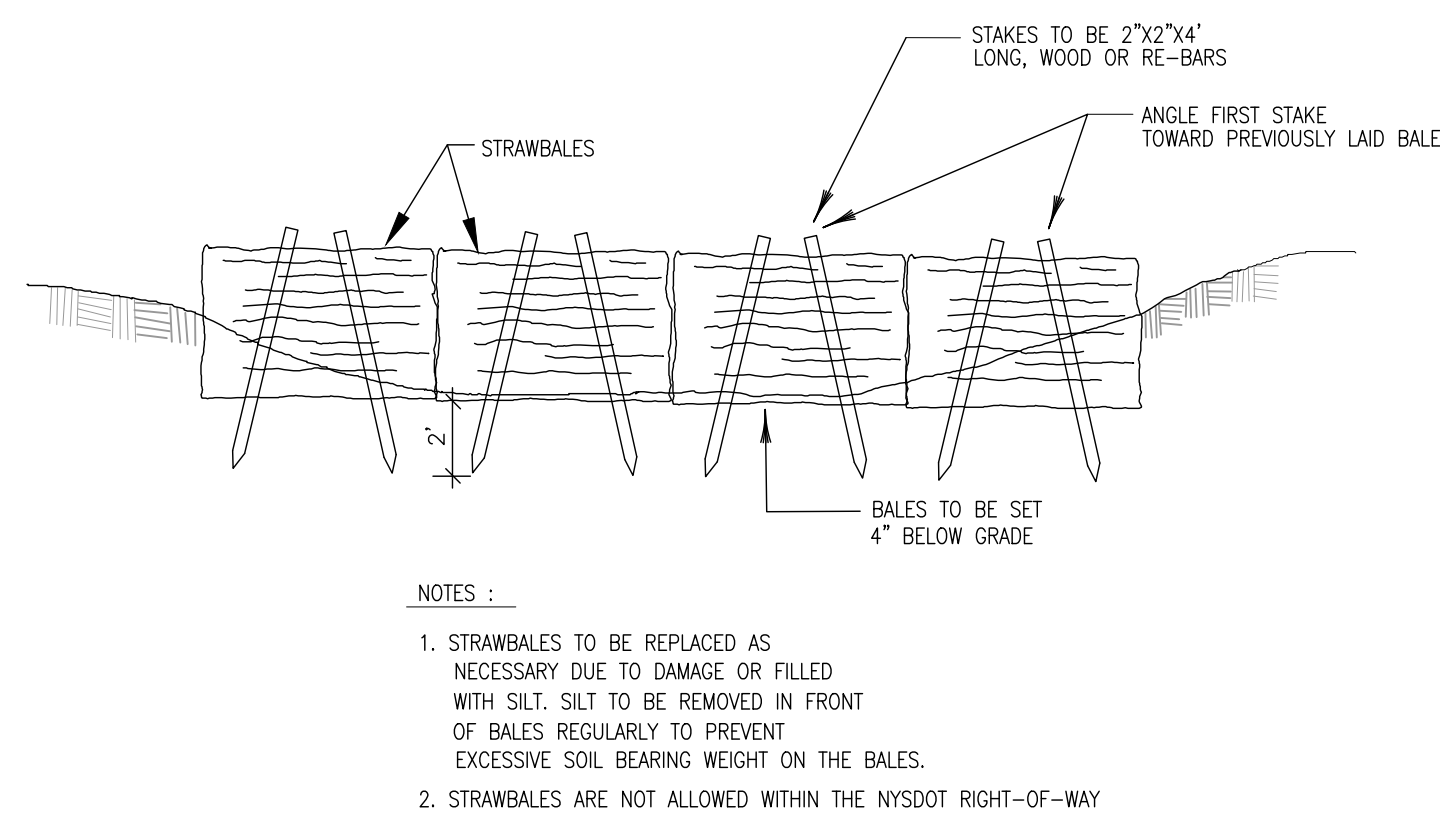
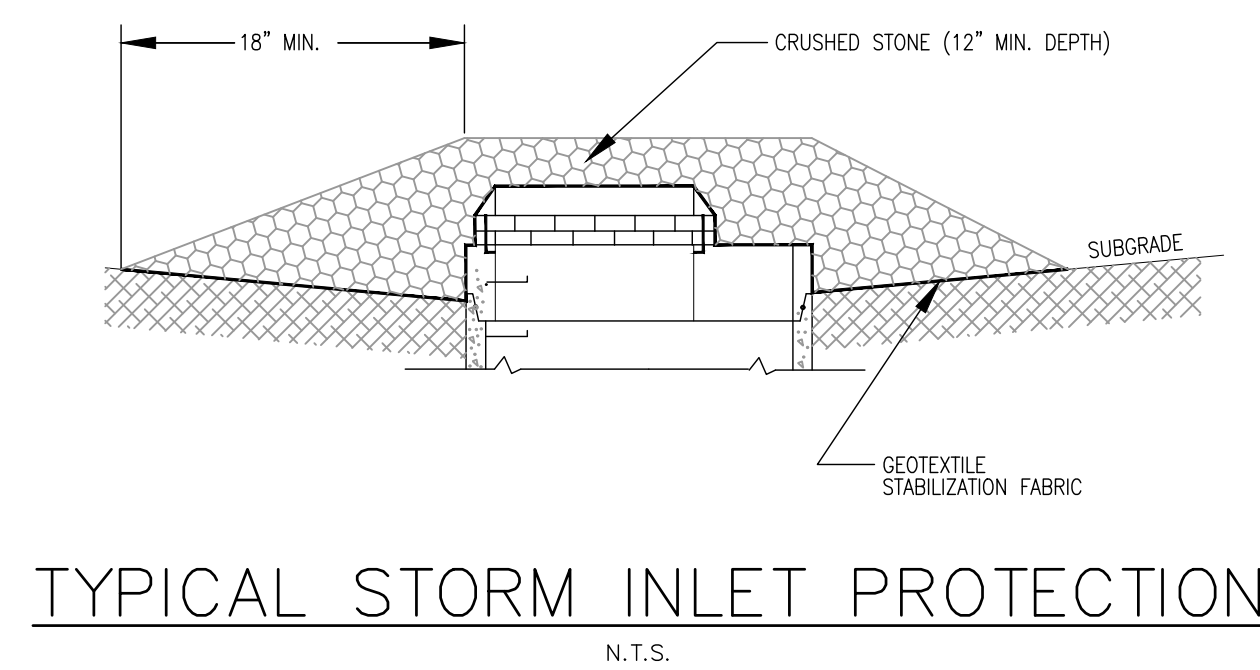
NOTES:

- STAKE ALL TREES 0'-4" CAL. OR LESS, AND EVERGREENS 5' - 12' IN HEIGHT. USE 8'-0" LONG STAKES DRIVEN 4'-0" BELOW GRADE.
- GUY ALL TREES LARGER 0'-4" CAL. AND EVERGREENS LARGER THAN 12'-0".





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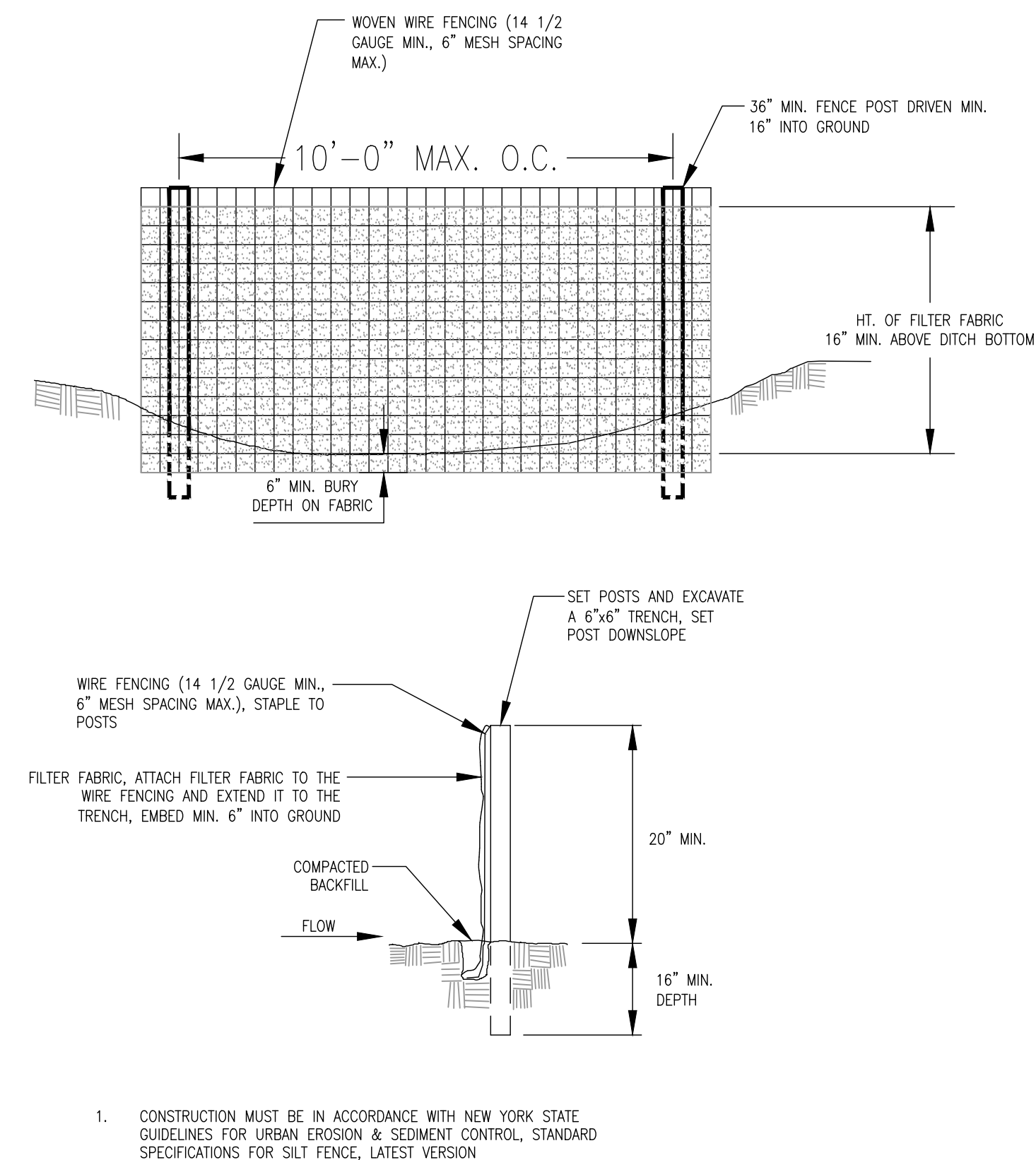
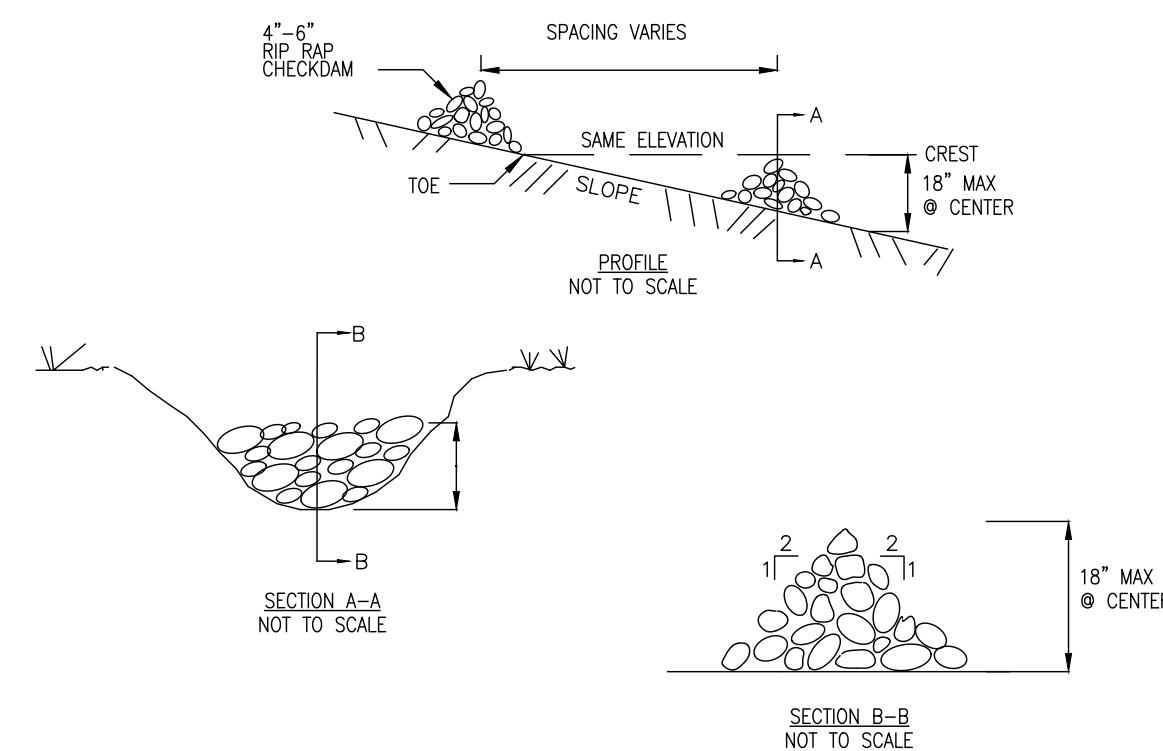
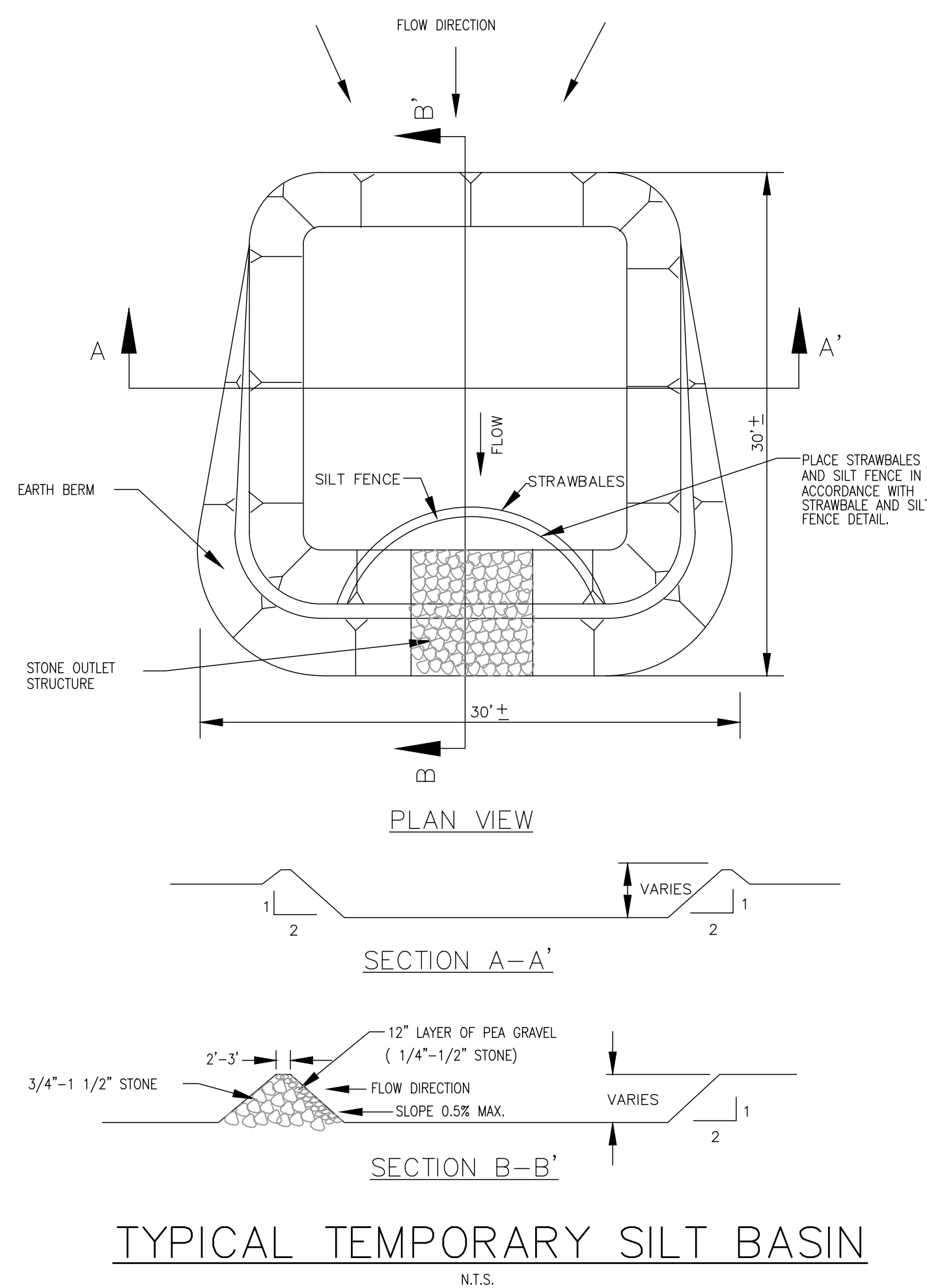


### STORMWATER POLLUTION PREVENTION NOTES

1. THE CONTRACTOR SHALL NOT HAVE MORE THAN FIVE (5) ACRES OF UNPROTECTED SOIL AT ONE TIME.
2. THE CONTRACTOR SHALL PROTECT ANY DISTURBED AREAS FROM EROSION WITHIN 14 DAYS OF DISTURBANCE.
3. EROSION CONTROL MEASURES - INCLUDING SILT FENCE - SHALL BE INSTALLED PRIOR TO THE START OF CONSTRUCTION.
4. THE CONTRACTOR SHALL INCORPORATE EROSION CONTROL MEASURES ON DISTURBED AREAS WITH SLOPES OF 3:1 OR GREATER OR IN CONCENTRATED FLOW PATHS. FOR DISTURBED AREAS WITH SLOPES OF 3:1 OR LESS, TEMPORARY MULCHING SHALL BE APPLIED AT AN APPLICATION RATE OF 100 LBS. PER 1000 SQ.FT.
5. THE CONTRACTOR SHALL PREPARE AND CONTINUOUSLY UPDATE A CONSTRUCTION PHASING PLAN FOR EROSION AND SEDIMENT CONTROL.
6. THE LOCATIONS FOR EQUIPMENT, WASTE, BORROW, AND OFF-SITE MATERIAL SHALL BE PROVIDED BY THE CONTRACTOR.
7. THE CONTRACTOR SHALL PROTECT SOIL AND STONE STOCKPILES FROM EROSION AT THE END OF EACH WORK DAY.
8. THE CONTRACTOR SHALL CONSTRUCT A STABILIZED CONSTRUCTION ENTRANCE AT STAGING AREAS TO PREVENT THE TRACKING OF SEDIMENT ON TO PUBLIC STREETS.
9. THE CONTRACTOR SHALL PERFORM WEEKLY AND POST RAINFALL INSPECTIONS OF ALL EROSION AND SEDIMENT CONTROL PRACTICES.
10. THE CONTRACTOR SHALL MAINTAIN EROSION AND SEDIMENT CONTROL PRACTICES UNTIL PERMANENT RESTORATION IS APPROVED BY AGENCY HAVING JURISDICTION.

NOTE:

- ANY AND ALL SILT FENCE AND EROSION CONTROL MEASURES SHALL BE INSTALLED PRIOR TO ANY AND ALL CONSTRUCTION ACTIVITIES, INCLUDING INSTALLATION OF SEWER MAINS AND LATERALS.



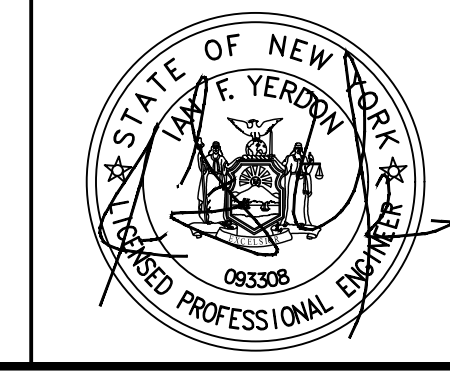
REVISIONS	REV DESCRIPTION DATE
FOR RECORD	AUGUST 2021

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DESIGNED BY: DMW  
CHECKED BY: DMW  
APPROVED BY: JFY